



CITY OF ALAMEDA

Standard Plans

December 2021

PUBLIC WORKS DEPARTMENT
City Hall West
950 West Mall Square, Suite 110
Alameda, CA 94501

CITY OF ALAMEDA

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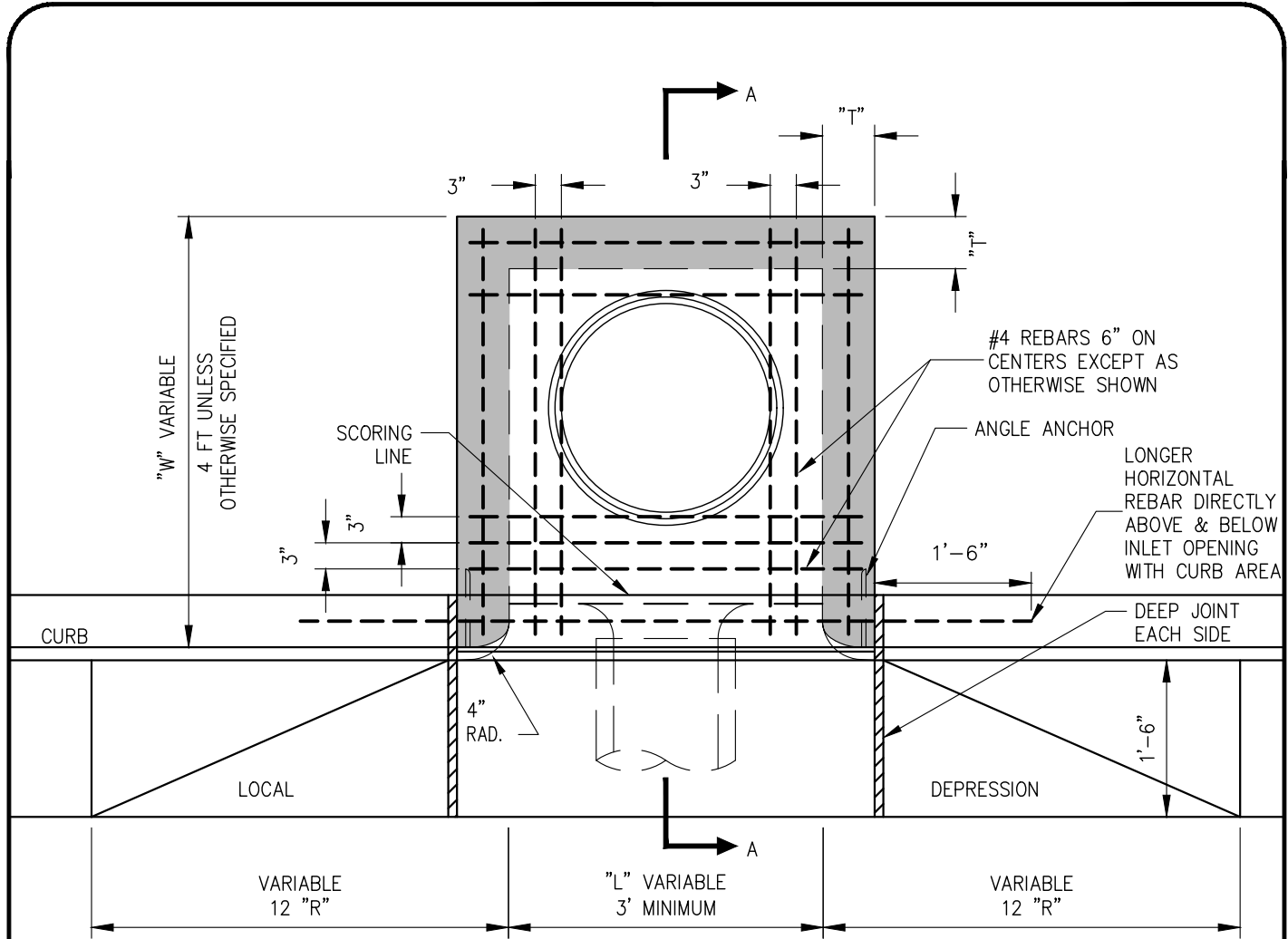
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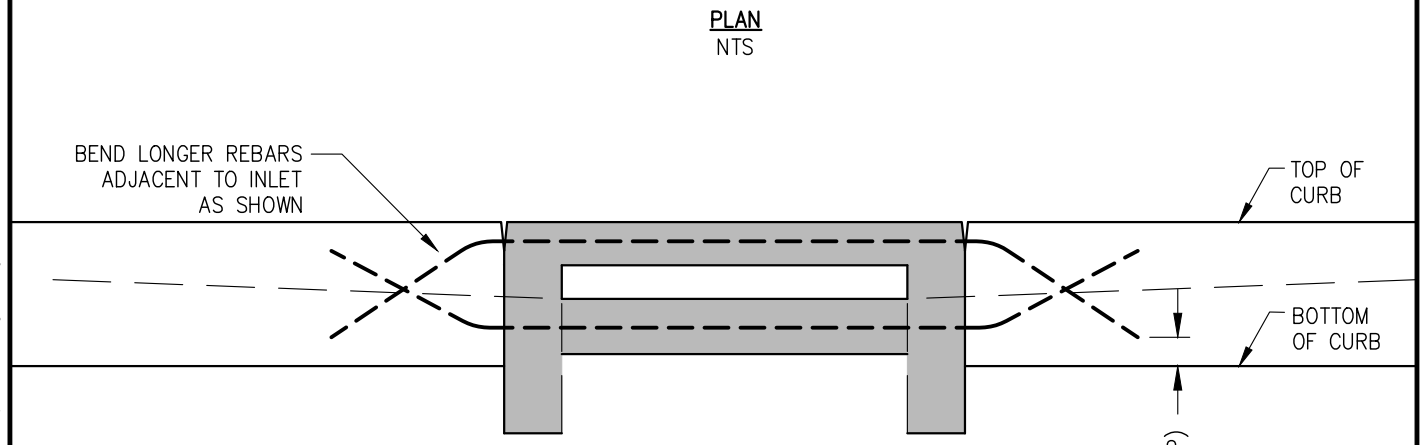
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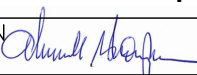


PLAN
NTS

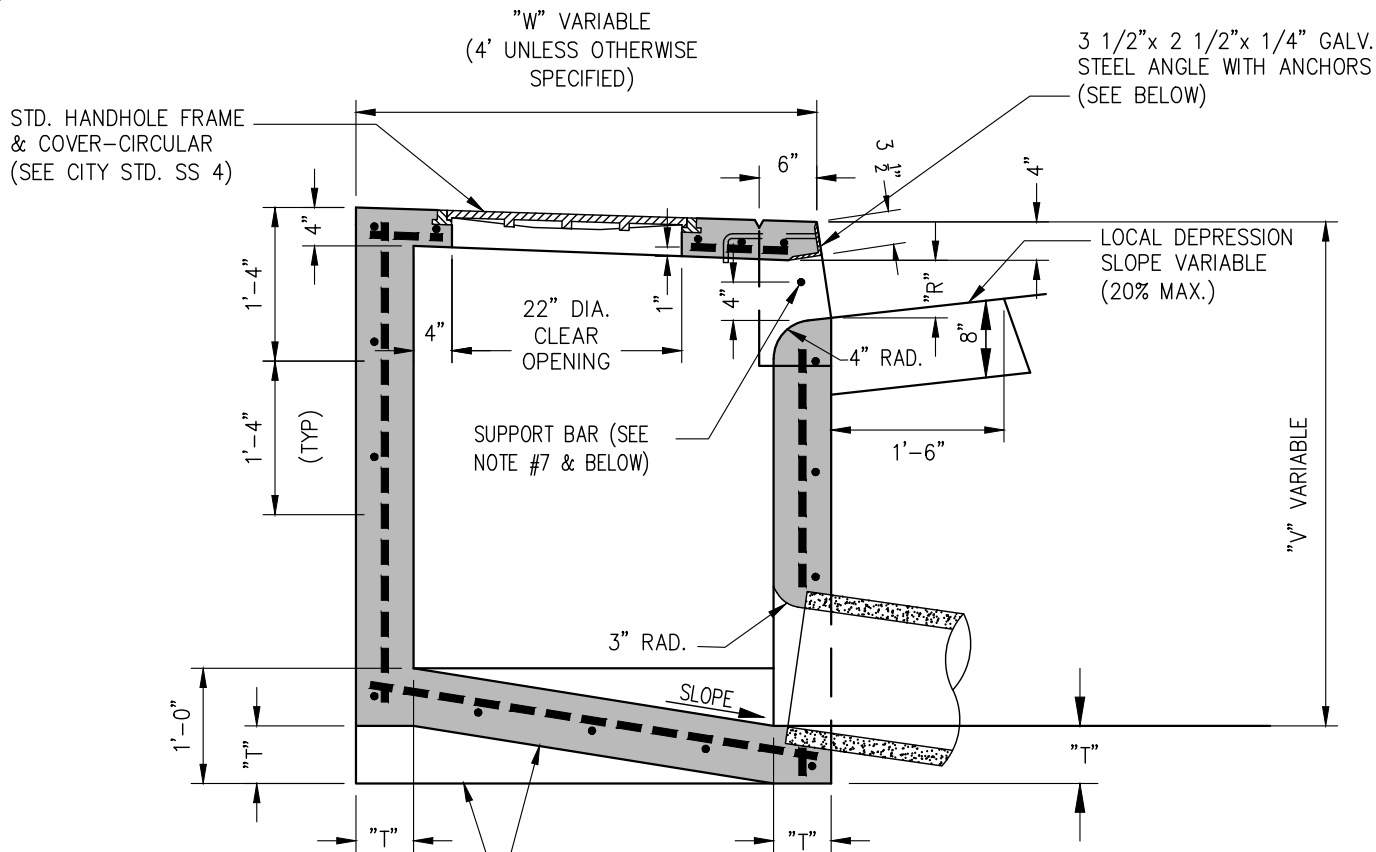


REBARS AT CURB OPENING
NTS

NO.	REVISED	BY	APP.

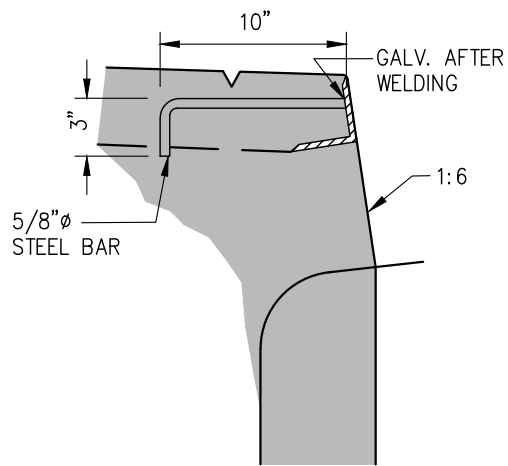
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	DATE 12/07/21		

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 1-2.dwg Layout Name: SD 1-2 Plot Date: Dec 07, 2021 at 12:08

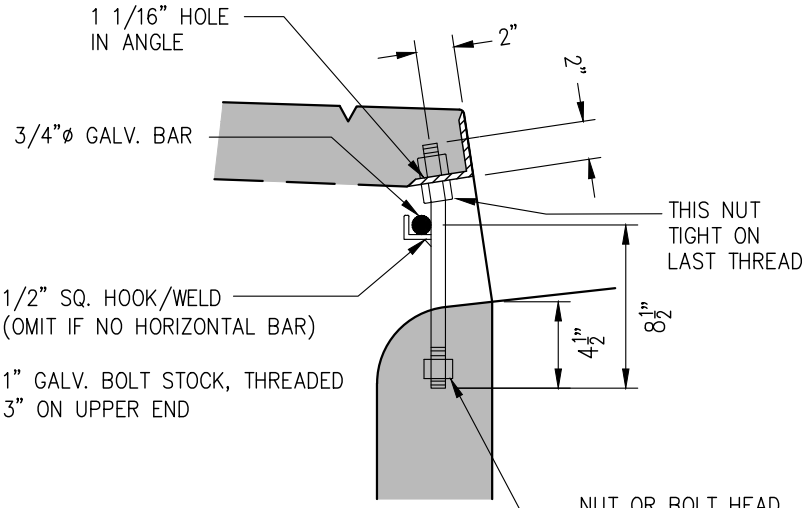


CONSTRUCT BASE ON LEVEL SUB-GRADE OR SLOPE SUB-GRADE (CONTRACTOR OPTION)

SECTION "A"
NTS




ANGLE ANCHOR
NTS



SUPPORT BAR
NTS

WALL THICKNESS	
"T"	"V"
6"	8' OR LESS
8"	EXCEEDS 8'

NO.	REVISED	BY	APP.


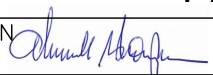
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	CATCH BASIN TYPE 1		SD 1-2
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NOTES:

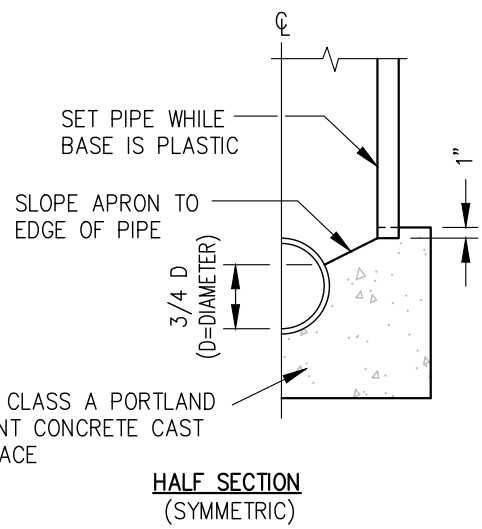
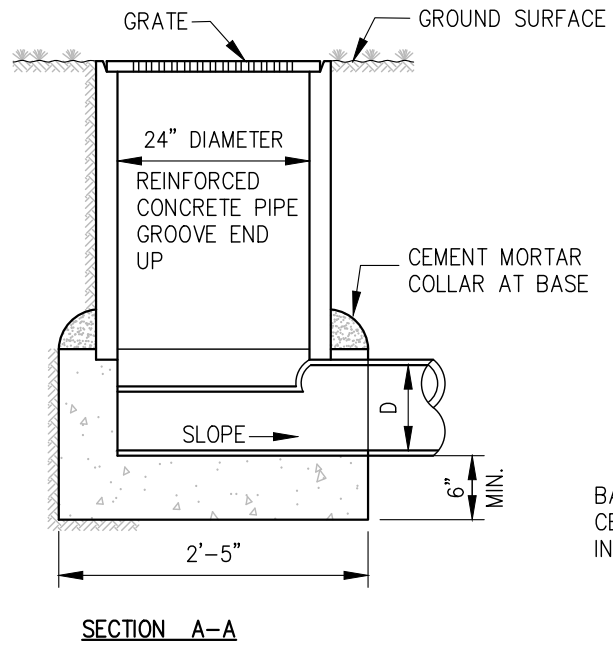
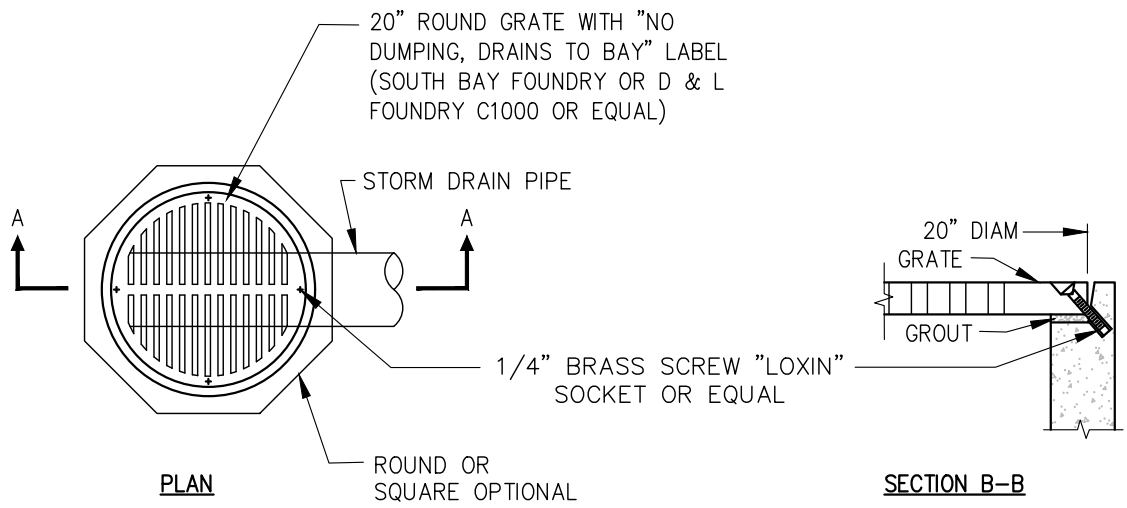
1. CONNECTION PIPES MAY BE PLACED IN ANY POSITION AROUND THE WALLS PROVIDED THEY POINT IN THE PROPER DIRECTION AND THE POSITION IS OTHERWISE CONSISTENT WITH THE IMPROVEMENT PLAN.
2. CURVATURE OF THE LIP AND SIDEWALLS AT GUTTER OPENING SHALL BE FORMED BY CURVED FORMS AND SHALL NOT BE MADE BY PLASTERING.
3. WALL THICKNESS (T)
 T = 6 INCHES IF H IS 8 FEET OR LESS
 T = 8 INCHES IF H EXCEEDS 8 FEET.
4. DEPTH (H) SHALL BE A MAXIMUM OF 6 FEET. FOR DEPTHS BETWEEN 6 FEET TO 12 FEET, CATCH BASIN SHALL BE ON A MAINTENANCE HOLE BASE, SEE SS 4. FOR DEPTHS GREATER THAN 12 FEET DEEP REQUIRE A SPECIAL DESIGN BY A REGISTERED CIVIL ENGINEER.
5. FLOOR OF BASIN SHALL BE TROWELED AND RETROWELED TO PRODUCE A HARD, POLISHED SURFACE OF MAXIMUM DENSITY AND SMOOTHNESS. SLOPE OF FLOOR PARALLEL WITH CURB SHALL BE 1 TO 12 UNLESS OTHERWISE SPECIFIED.
6. MAINTENANCE HOLE SHALL BE PLACED AS SHOWN IN IMPROVEMENT PLANS.
7. OUTLET PIPE SHALL BE TRIMMED TO THE FINAL SHAPE AND LENGTH BEFORE CONCRETE IS POURED.
8. REINFORCING STEEL SHALL BE #4 ROUND DEFORMED BARS. SPLICES SHALL BE 10" MIN. LAPS (INCLUDING CORNERS)
9. CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2 "MINOR CONCRETE" OF STATE STANDARD SPECIFICATION, 3/4" AGGREGATE.
10. STEPS ARE NOT ALLOWED.
11. SURFACE OF ALL EXPOSED CONCRETE IN BASIN SHALL CONFORM IN SLOPE, GRADE, COLOR, FINISH, AND SCORING TO EXISTING OR PROPOSED CURB AND WALK ADJACENT TO THE BASIN.
12. EXCEPTION WHEN THE BASIN IS TO BE CONSTRUCTED WITHIN THE LIMITS OF A PROPOSED SIDEWALK OR IS CONTIGUOUS TO SUCH A SIDEWALK, THE TOP OF THE BASIN SHALL BE POURED MONOLITHIC WITH THE SIDEWALK, USING THE SAME CLASS OF CONCRETE AS IN THE SIDEWALK. IN THIS CASE THE DOWELS BETWEEN WALL AND TOP OF SLAB SHALL BE OMITTED AND THE TOP OF THE CATCH BASIN WALL FINISHED SMOOTH.
13. GENERAL NOTES: CATCH BASIN DETAILS SHOWN ARE FOR CAST-IN-PLACE. PRECAST OR PREFORMED ALTERNATES MAY BE APPROVED AT THE DISCRETION OF THE CITY ENGINEER. (OLDCASTLE PRE-CAST MODEL 3AC/4AC, JENSEN SACRAMENTO COUNTY TYPE G, OR SIMILAR.)
14. HORIZONTAL PROTECTION BAR SHALL BE USED WHEN CURB FACE IS 10 INCHES OR MORE.
15. WHEN "L" EXCEEDS 3'-6", VERTICAL SUPPORT BARS SHALL BE USED AT 42" MAX. SPACING.
16. "NO DUMPING - DRAINS TO BAY" LABELING.

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 1-3.dwg
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NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	CATCH BASIN TYPE 1	SD 1-3
	DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 2.dwg Layout Name: SD 2 Plot Date: Dec 07, 2021 at 12:14



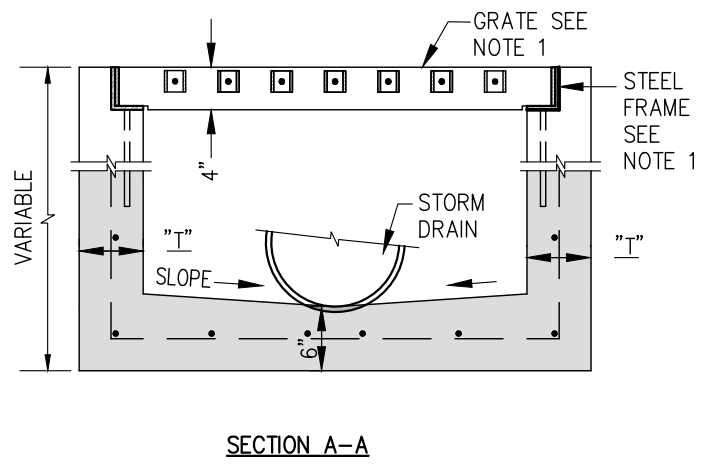
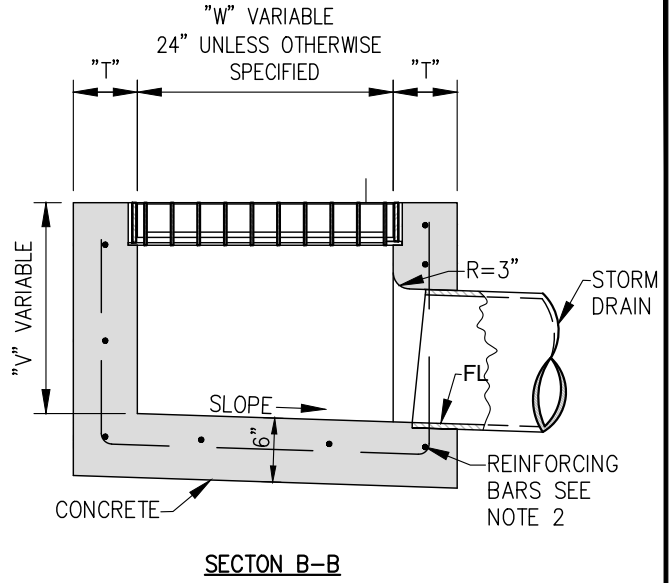
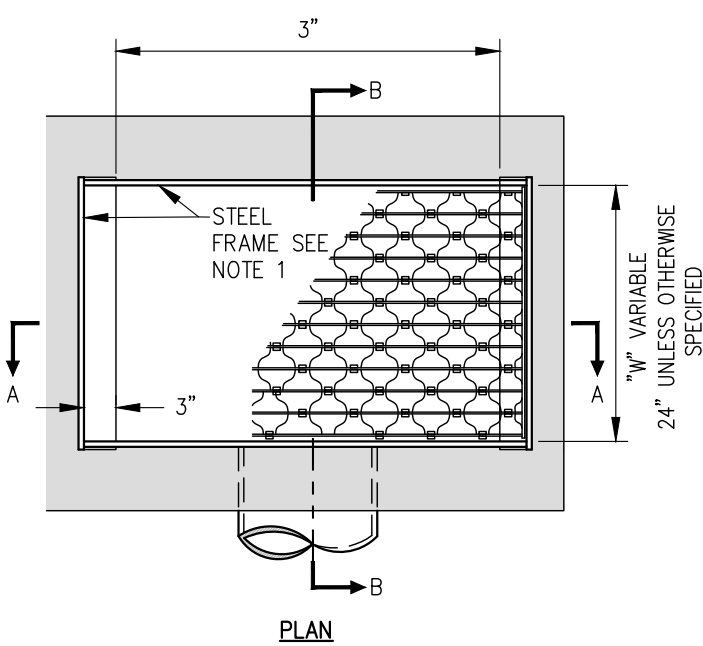
NOTE:

1. FOR USE IN CITY PARKS OR ON PRIVATE PROPERTY ON A CASE BY CASE BASIS PER APPROVED PLANS. PRECAST CONCRETE CONSTRUCTION OF SAME OR LARGER DIMENSIONS IS ACCEPTABLE (E.G. JENSEN PRECAST MODEL NO. D1203NB DROP INLET OR SIMILAR).
2. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2 "MINOR CONCRETE" OF STATE SPECIFICATIONS, 3/4" AGGREGATE.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	CATCH BASIN TYPE A		SD 2
	DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21	PAGE 1 OF 1 SCALE: NONE


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 3.dwg Layout Name: SD 3 Plot Date: Dec 07, 2021 at 12:16



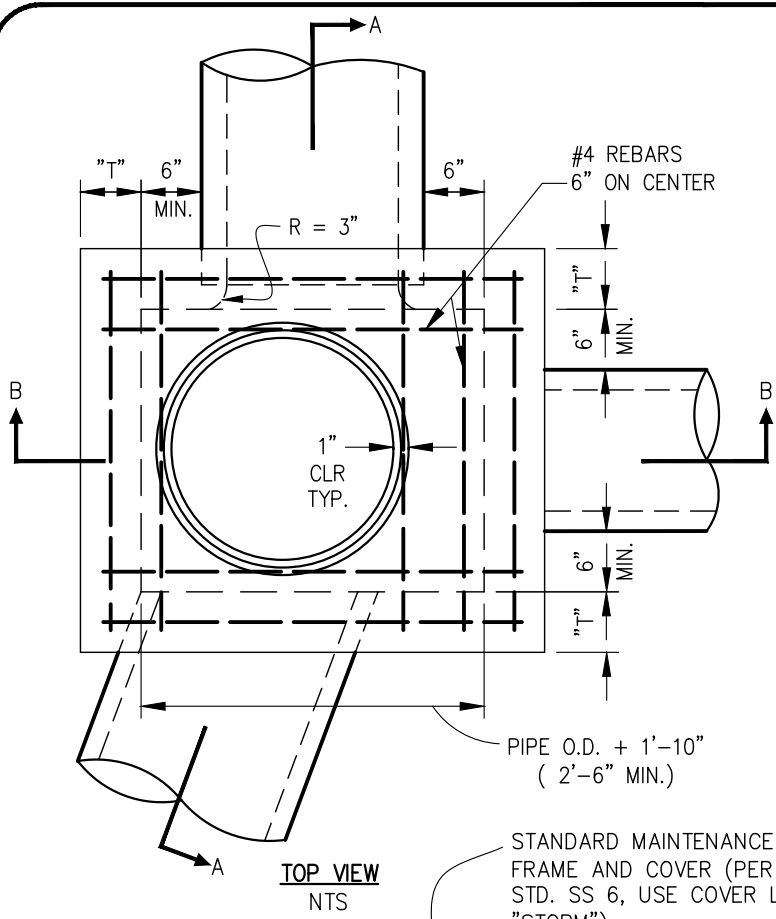
- NOTES:**
- FOR 24" W FRAME AND GRATE SHALL BE GALVANIZED STEEL PHOENIX IRON WORKS P-6302 OR APPROVED EQUAL WHERE "V" IS GREATER THAN 8', "W" SHALL BE 36" WITH FRAME & GRATE INWESCO (36"X36" CLEAR OPENING) OR APPROVED EQUAL.
 - REINFORCING BARS SHALL BE #4 BARS AT 9" O.C.
 - SLOPE BOTTOM EACH WAY 0.10' MINIMUM TO THE OUTFALL OR AS OTHERWISE SPECIFIED.
 - STEPS ARE NOT ALLOWED.
 - "NO DUMPING - DRAINS TO BAY" LABELING ON LID.
 - PRECAST, REINFORCED CONCRETE SECTIONS WITH TONGUE AND GROOVE, CASKETED JOINTS ARE ACCEPTABLE (OLDCASTLE OR JENSEN PRECAST TYPE G1).
 - CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2 "MINOR CONCRETE" OF STATE SPECIFICATIONS, 3/4" AGGREGATE.

WALL THICKNESS	
"T"	"V"
6"	8' OR LESS
8"	EXCEEDS 8'

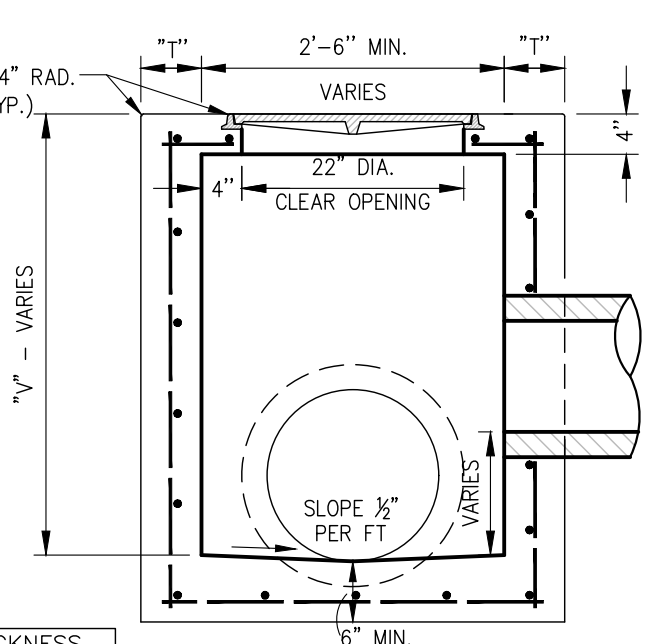
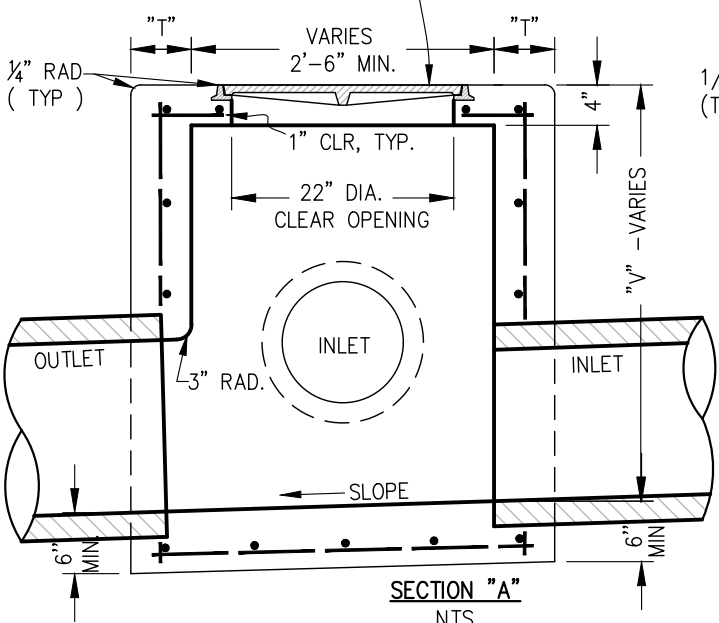
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	GRATED CATCH BASIN		SD 3
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21

Images: Alameda.png; RST Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 4.dwg Layout Name: SD 4 Plot Date: Dec 07, 2021 at 12:17




- NOTES:**
1. THIS STRUCTURE NOT FOR STREET AREA USE. STREET USE REQUIRES ADDITIONAL REINFORCEMENT PER CITY ENGINEER.
 2. STEPS SHALL BE USED WHEN DEPTH IS GREATER THAN 3'-6". VERTICAL SPACING AT 16" CENTERS. STEPS SHALL BE 1/2" Ø STEEL, POLYPROPYLENE PLASTIC COATED.
 3. CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2 "MINOR CONCRETE" OF STATE SPECIFICATIONS, 3/4" AGGREGATE.
 4. EXPOSED SURFACES TO CONFORM TO ADJOINING CURB AND WALK IN GRADE & FINISH.
 5. WALLS AND FLOOR SHALL BE REINFORCED WITH #3 BARS AT 9" C-C. ROOF SHALL BE REINFORCED WITH #4 BARS AS SHOWN. ALL SPLICES IN REINFORCING SHALL HAVE 10" MIN. LAP (INCLUDING AT CORNERS). BARS SHALL BE PLACED 2 1/2" CLEAR FROM INTERIOR FACES EXCEPT AS NOTED.
 6. INLET & OUTLET LOCATIONS AS SPECIFIED.
 7. PRECAST, REINFORCED CONCRETE SECTIONS WITH TONGUE AND GROOVE, GASKETED JOINTS MAY BE APPROVED AT THE DISCRETION OF CITY ENGINEER.

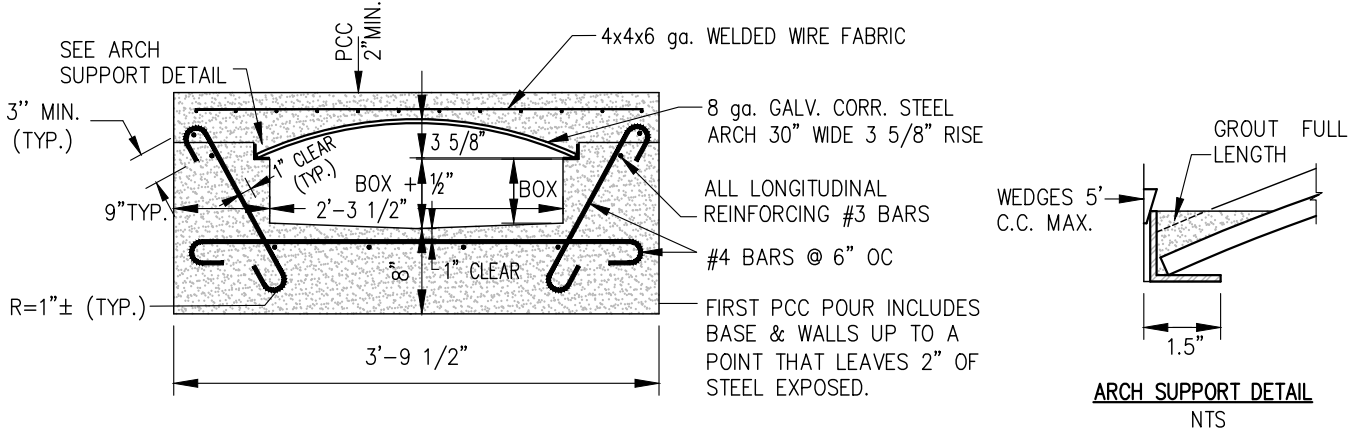
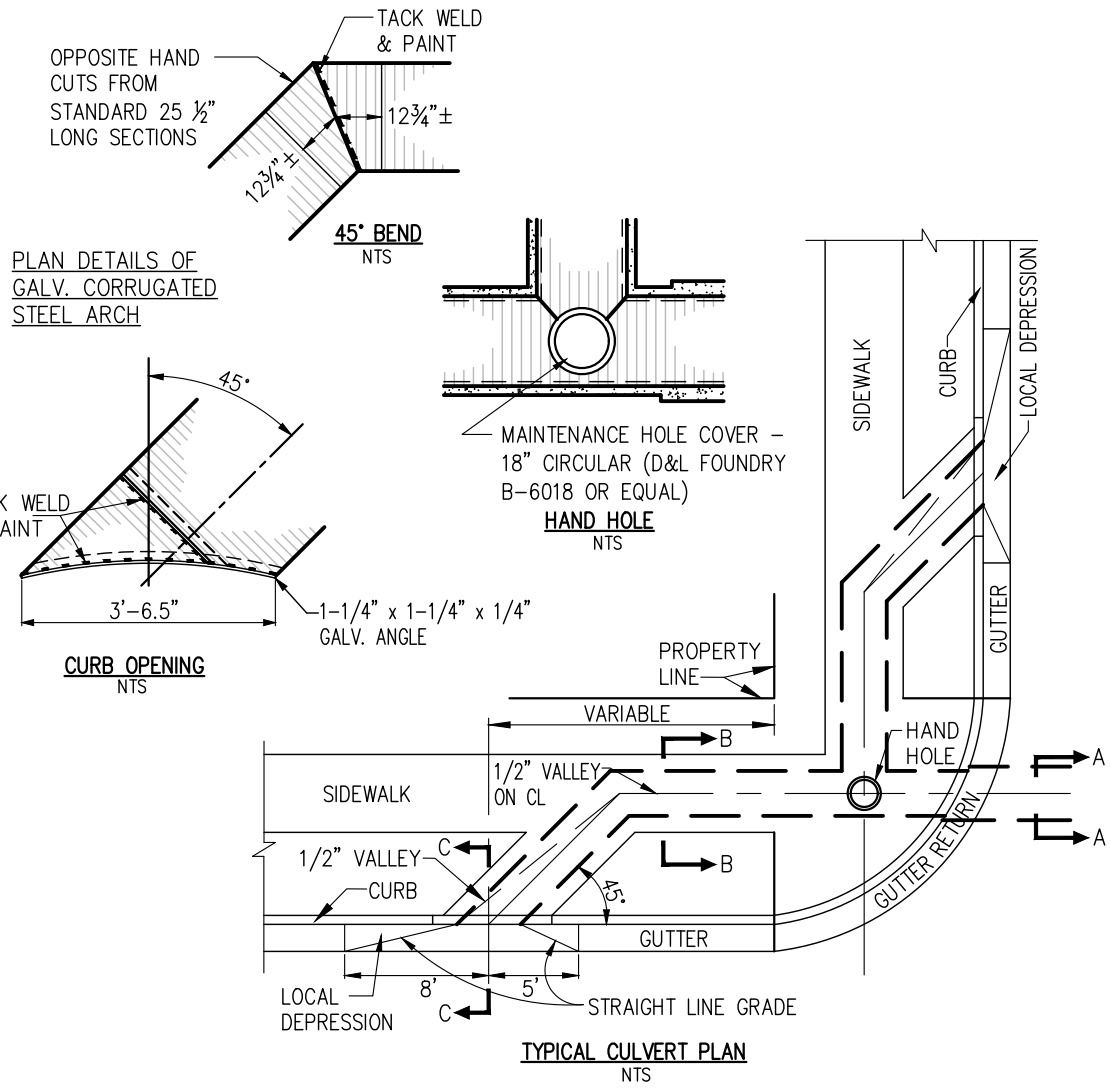


NO.	REVISED	BY	APP.


WALL THICKNESS	
"T"	"V"
6"	8' OR LESS
8"	EXCEEDS 8'

	DWN: CLG CHK JT DEC 2021	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT MAINTENANCE HOLE FOR STORM DRAIN RUSS THOMPSON CITY ENGINEER	STD DETAIL SD 4 PAGE 1 OF 1 SCALE: NONE
	DATE 12/07/21		(Signature)
	(Signature)		

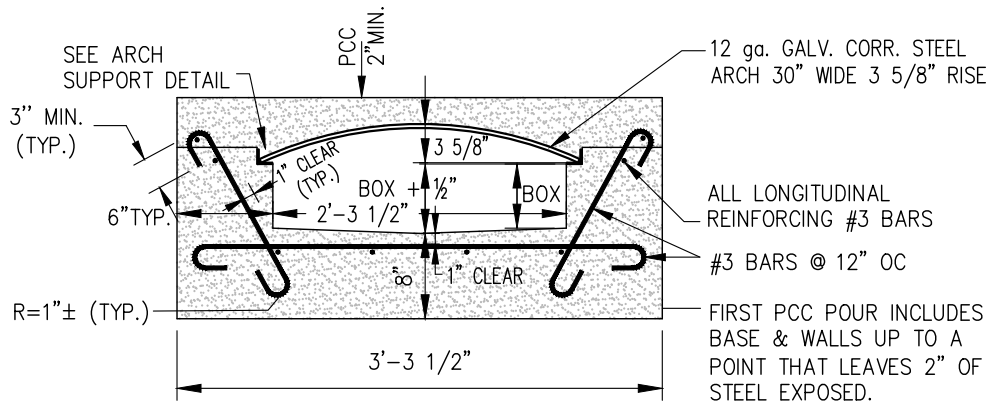
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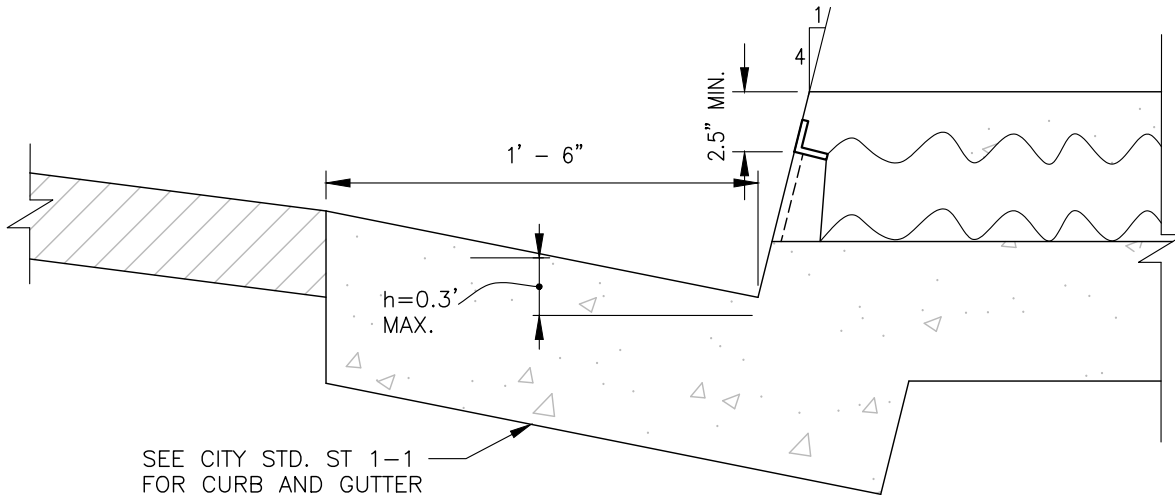
NO.	REVISED	BY	APP.

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	CULVERT - 30 INCH	SD 5-1
	DWN: CLG CHK JT DEC 2021 RUSS THOMPSON CITY ENGINEER	DATE 12/07/21 SCALE: NONE

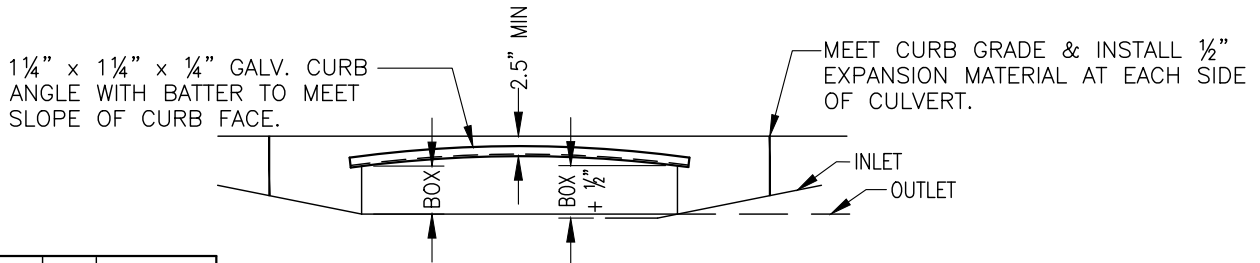
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STREET SECTION B-B
NTS


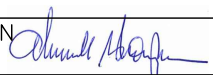


SECTION C-C
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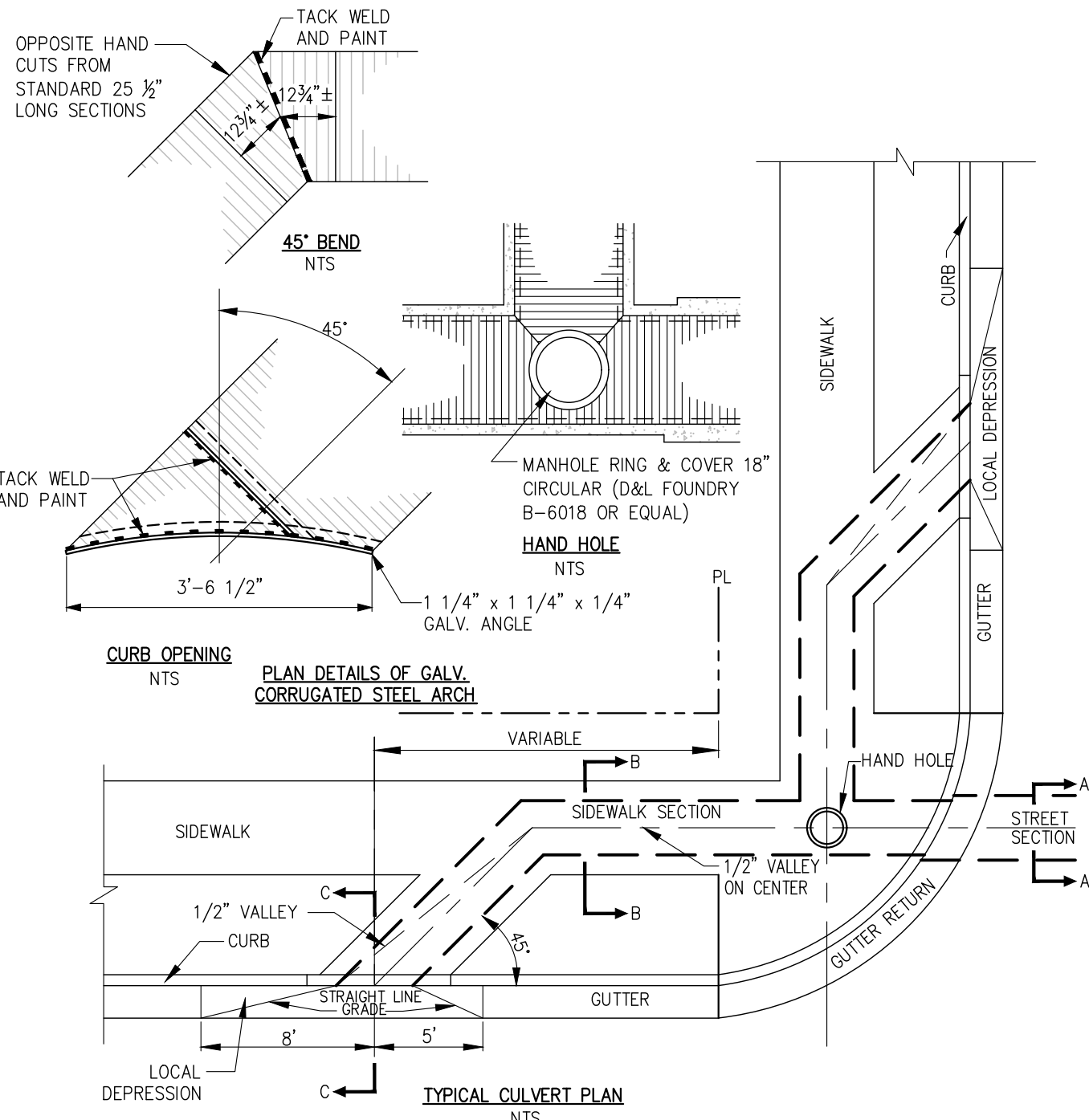


CURB OPENING
NTS


NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DWN: CLG	CULVERT - 30 INCH	SD 5-2
	CHK JT		PAGE 2 OF 2
DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21	SCALE: NONE

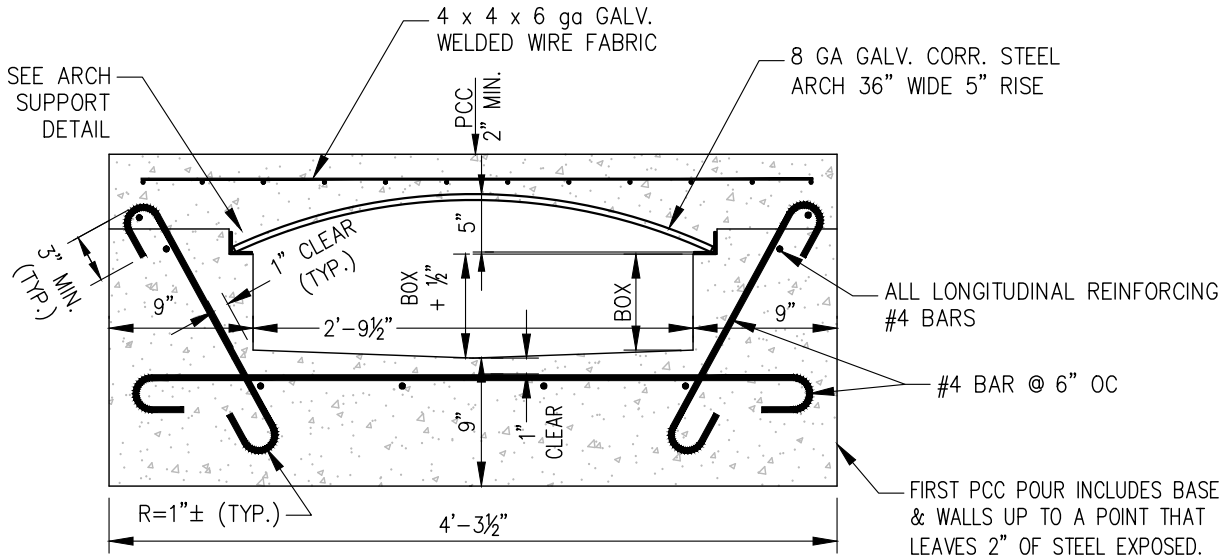
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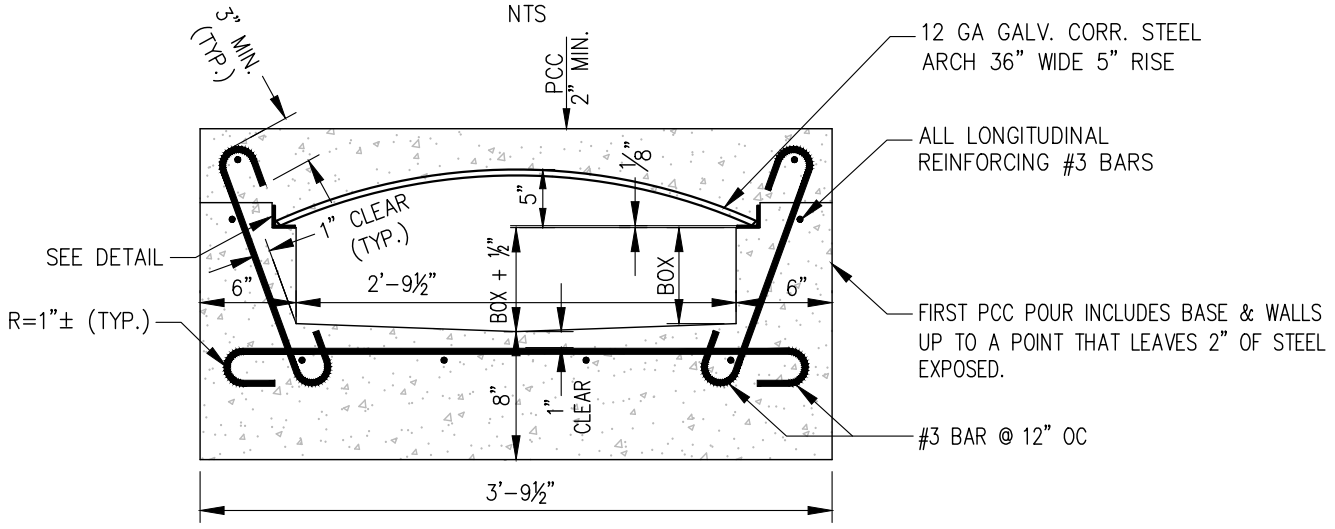
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	CHK JT		SD 6-1
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21

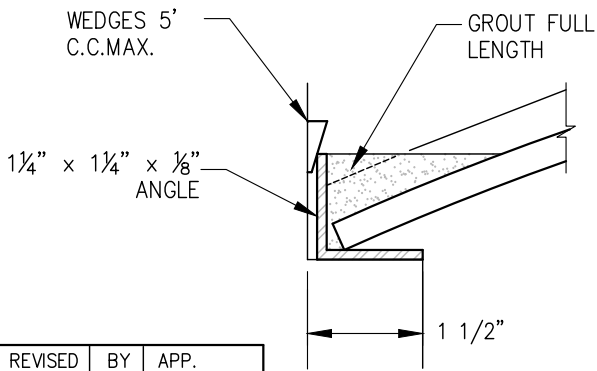
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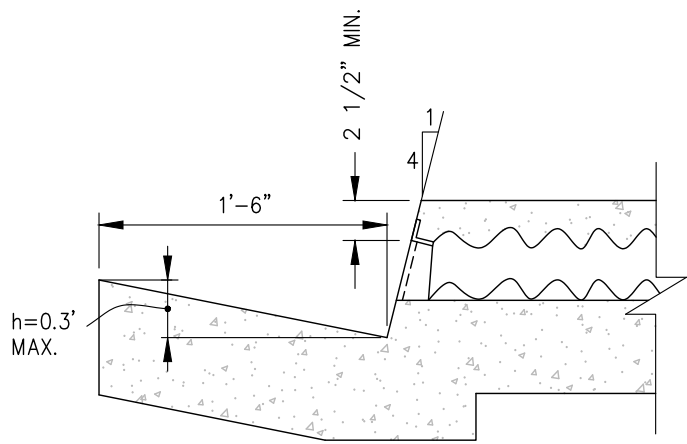
SECTION A-A
NTS



SECTION B-B
NTS



ARCH SUPPORT
NTS



SECTION C-C
NTS

NO.	REVISED	BY	APP.

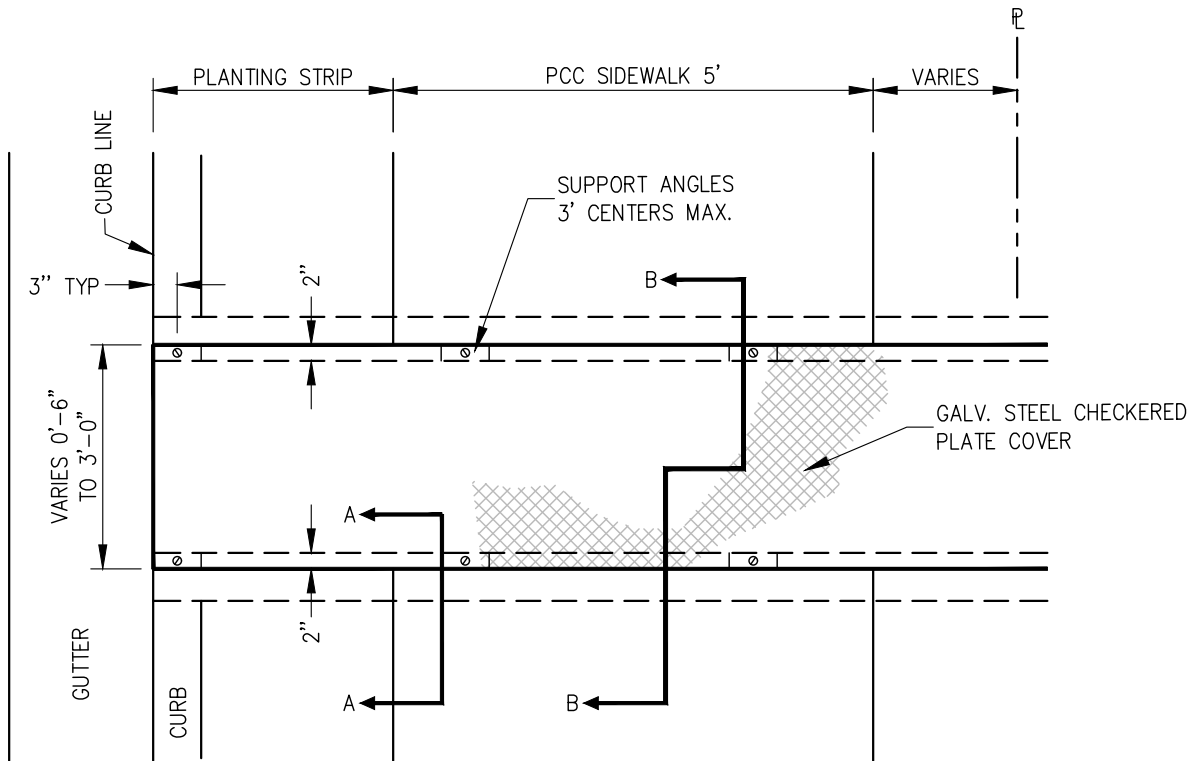


DWN: CLG
CHK JT
DEC 2021

CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT
CULVERT - 36 INCH
RUSS THOMPSON
CITY ENGINEER

STD DETAIL
SD 6-2
PAGE 2 OF 2
SCALE: NONE
DATE 12/07/21


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 7-1.dwg Layout Name: SD 7-1 Plot Date: Dec 07, 2021 at 12:29



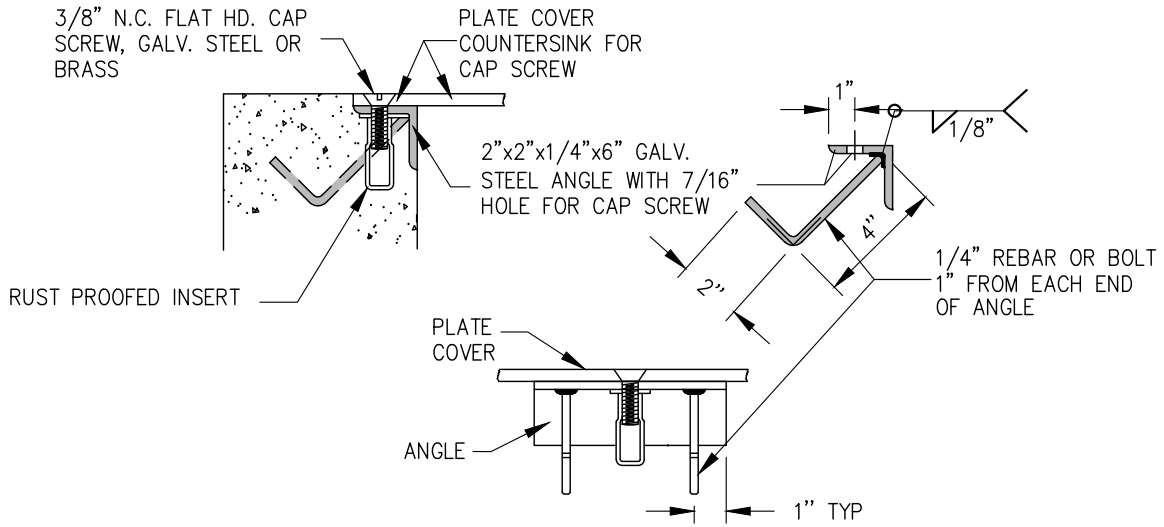
PLAN
NTS

CHECKERED PLATE COVER THICKNESS		
INTERIOR WIDTH OF CHANNEL	DRIVEWAY AREA	SIDEWALK AREA
6" TO 1'-6"	5/8"	3/8"
1'-7" TO 2'-0"	11/16"	
2'-1" TO 3'-0"	3/4"	

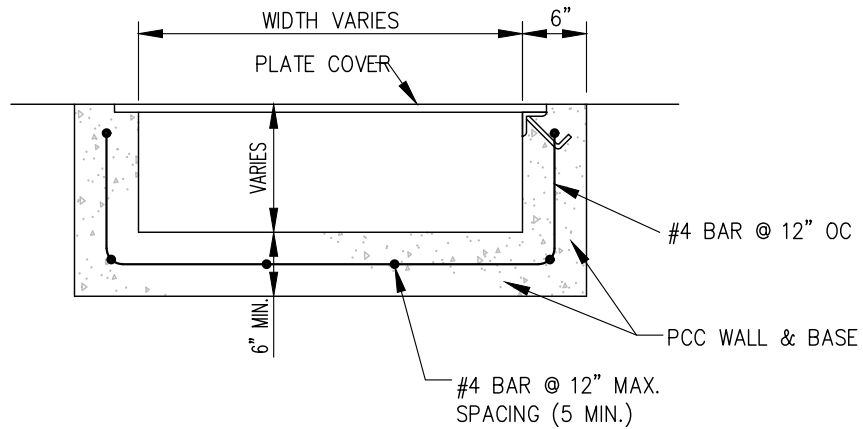
NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DRAINAGE CHANNEL		SD 7-1
	DWN: CLG CHK JT	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 7-2.dwg Layout Name: SD 7-2 Plot Date: Dec 07, 2021 at 12:30



SECTION A-A
NTS

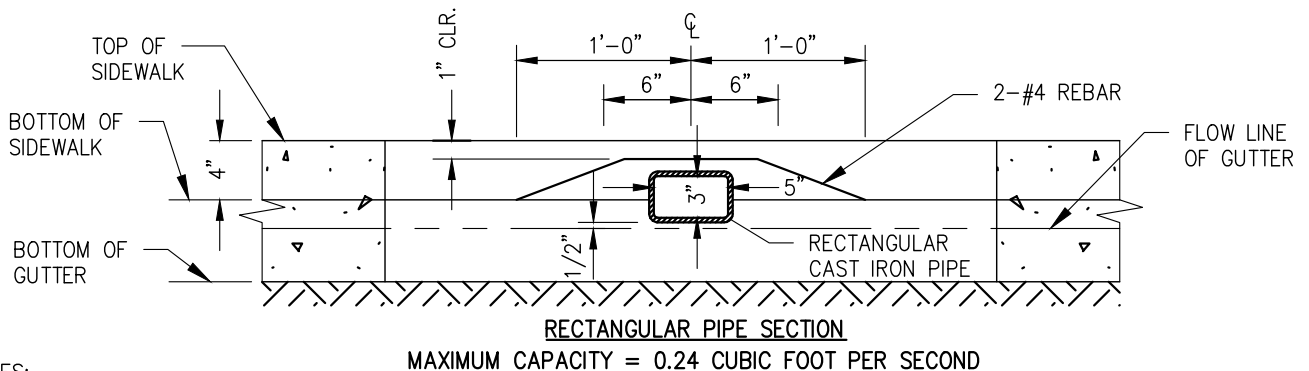
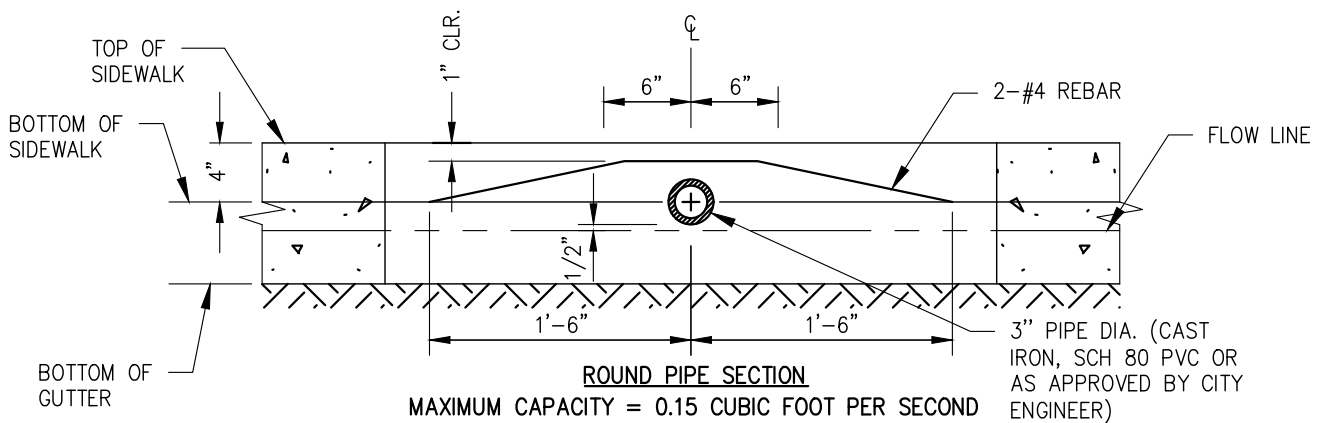
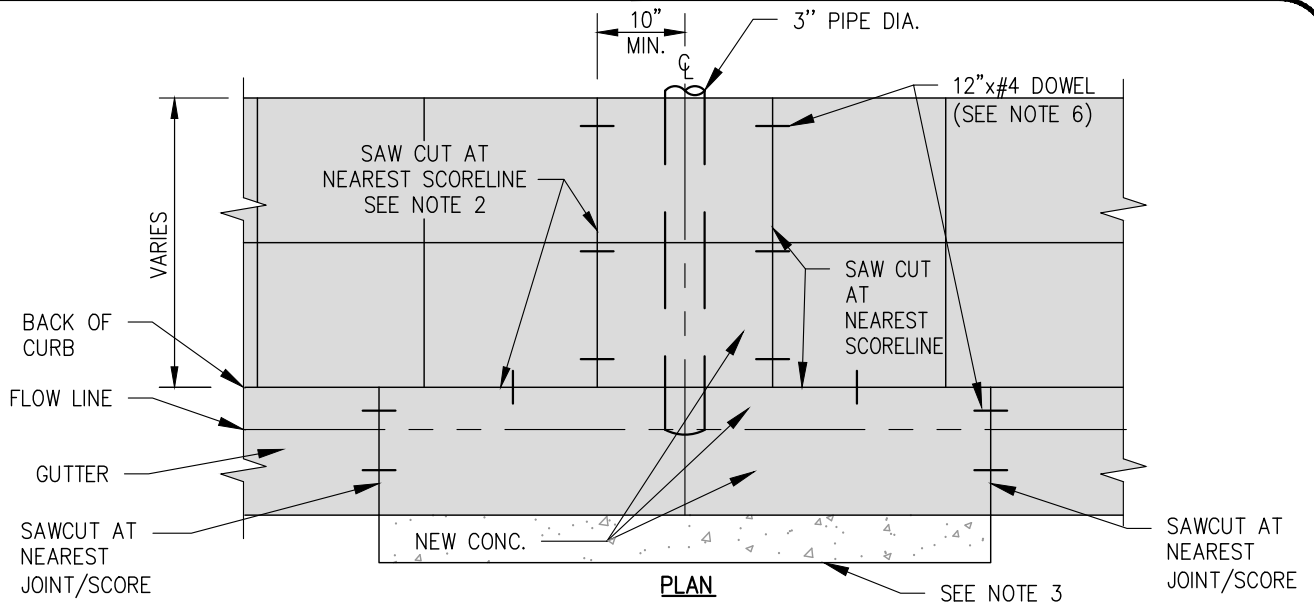


SECTION B-B
NTS

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DRAINAGE CHANNEL		SD 7-2
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SD 8.dwg Layout Name: SD 8 Plot Date: Dec 07, 2021 at 12:31



NOTES:

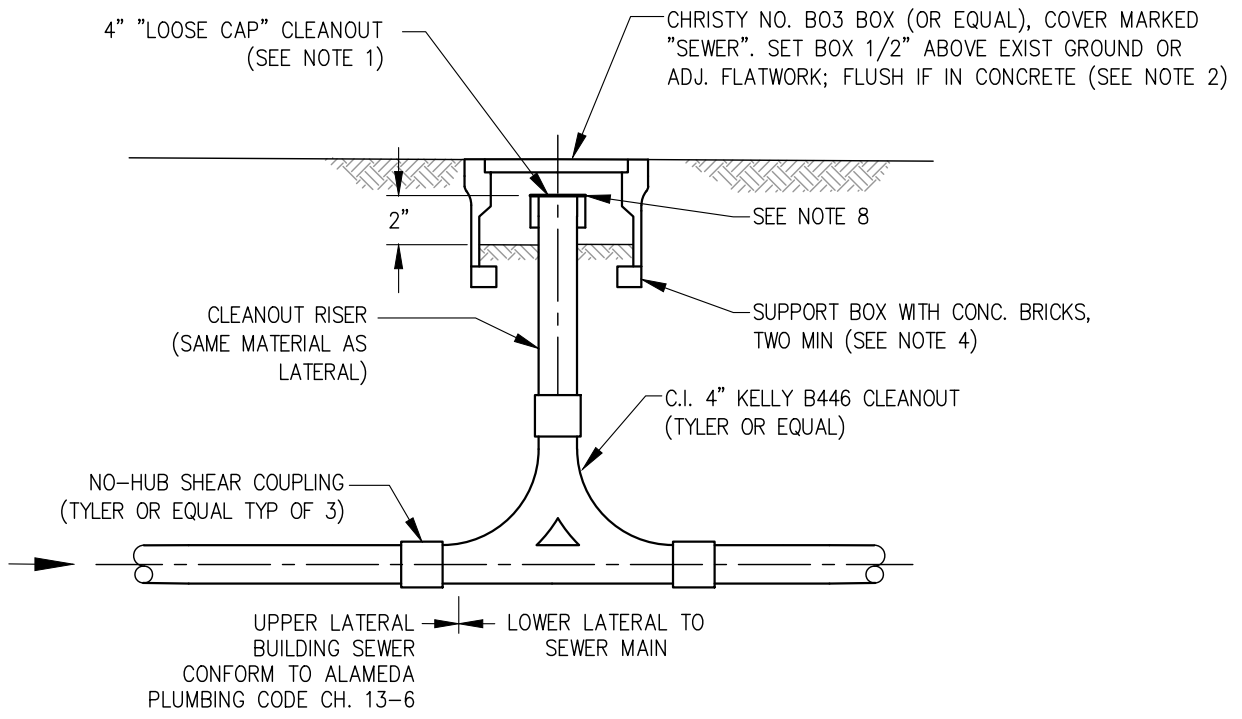
1. PRIOR TO BEGINNING ANY WORK, AN ENCROACHMENT PERMIT SHALL BE OBTAINED.
2. THE NUMBER OF OUTLET PIPES IS SUBJECT TO REVIEW BY THE CITY ENGINEER.
3. REFER TO ST 1-1 FOR CURB & GUTTER DETAILS.
4. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2, "MINOR CONCRETE" OF STATE SPECIFICATION. 3/4" AGGREGATE WITH 1 1/2 POUNDS OF LAMP BLACK PER CUBIC YARD. FINISH TO BE UNIFORM MEDIUM BROOMED TEXTURE.
5. MINIMUM SLOPE OF DRAIN PIPE TO BE 1%.
6. 12" #4 REBAR DOWLS, 6" MINIMUM DEPTH INTO EXISTING SIDEWALK, CURB & GUTTER. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS. PIPE MATERIAL TO BE SCH 80 PVC, CAST IRON, OR AS APPROVED BY CITY ENGINEER.

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CURB DRAIN IN EXISTING CURB AND SIDEWALK	SD 8
	DWN: CLG CHK JT DEC 2021 RUSS THOMPSON CITY ENGINEER	PAGE 1 OF 1 SCALE: NONE

DATE 12/07/21


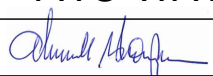
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NOTES:

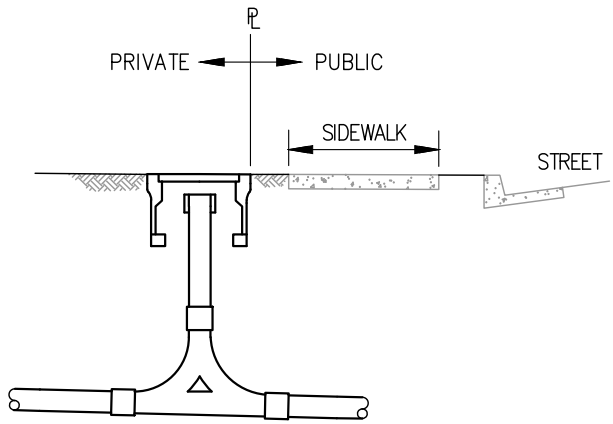
1. INSTALL CLEANOUTS ON ALL 4" SEWER LATERAL SERVICES. INSTALL "LOOSE CAP" USING RUBBER, STAINLESS STEEL BANDED END CAP, LOOSELY TIGHTENED TO ALLOW FLUID BACK PRESSURE TO REMOVE THE CAP, OR SEWER POPPER, OR SIMILAR.
2. IN DRIVEWAYS: USE CHRISTY B03C CAST-IRON LID OR EQUAL; IN LAWN/LANDSCAPE AREAS OR SIDEWALKS NOT SUBJECT TO TRAFFIC LOADING: USE CHRISTY B03D REINFORCED CONCRETE LID. IN DRIVEWAY AREAS, ANGLE BOX TO MATCH DRIVEWAY SLOPE.
3. INSTALL 2-WAY CLEANOUTS IF REQUIRED PER PLANS AND AT PROPERTY/RIGHT-OF-WAY LINE.
4. SUPPORT NOT NEEDED IF SET IN CONCRETE.
5. PROVIDE OVERFLOW PROTECTION OR BACKWATER DEVICE IF THE DIFFERENCE IN ELEVATION BETWEEN THE LOWEST FLOOR WITH PLUMBING WASTE FIXTURES OR FLOOR DRAINS AND THE RIM OF THE NEAREST UPSTREAM MANHOLE OR CLEANOUT CAP IS 12" OR LESS.
6. PROVIDE ADDITIONAL CLEANOUTS IF GREATER THAN 100 FT SPACING OR FOR EACH AGGREGATE HORIZONTAL CHANGE OF DIRECTION EXCEEDING 135 DEGREES.
7. FOR CLEANOUT LOCATION SEE DETAIL ON SHEET 2.
8. CLEARANCE FROM TOP OF CHRISTY BOX TO TOP OF CAP SHALL BE 5" MINIMUM.

NO.	REVISED	BY	APP.

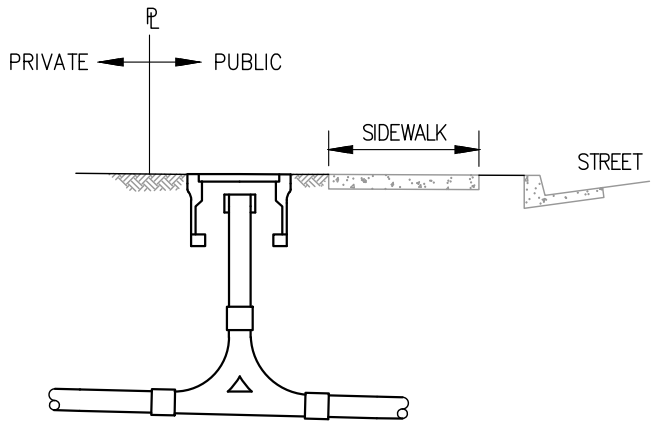
	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	4" SEWER LATERAL TWO-WAY CLEANOUT	SS 1-1
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER 

DATE 12/07/21

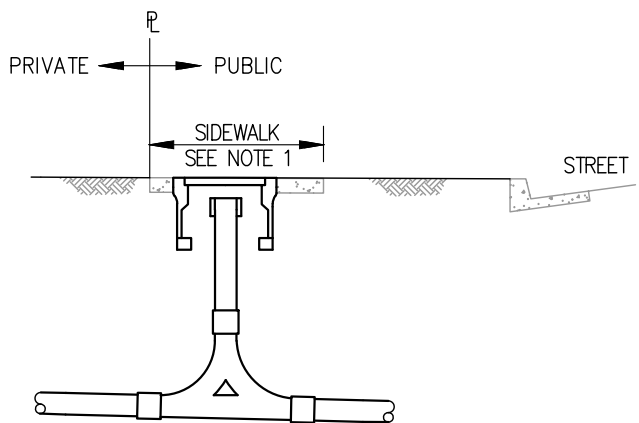
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**STREET SERVICE
BEHIND THE SIDEWALK
ON PRIVATE PROPERTY**
NTS

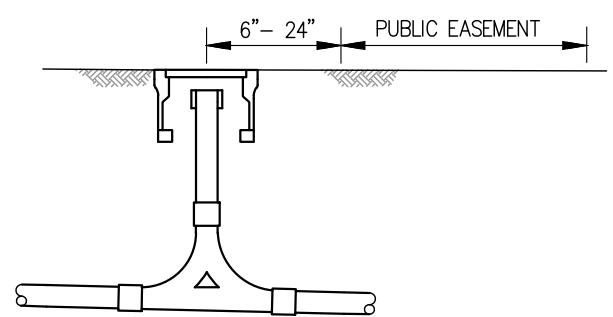


**STREET SERVICE
BEHIND THE SIDEWALK
IN PUBLIC RIGHT OF WAY**
NTS



WITH APPROVAL OF THE CITY, CLEANOUTS MAY BE INSTALLED IN SIDEWALK WHERE A WALL OR OTHER STRUCTURE PREVENTS INSTALLATION BEHIND WALK. **RIGHT OF WAY CONCRETE PERMIT IS REQUIRED.**

**STREET SERVICE
SIDEWALK OR DRIVEWAY**
NTS




CLEANOUTS ON SERVICES AT EASEMENTS INSTALLED 6" TO 24" OUTSIDE THE EASEMENT. **CONTACT PUBLIC WORKS FOR LOCATION OF PUBLIC EASEMENT.**

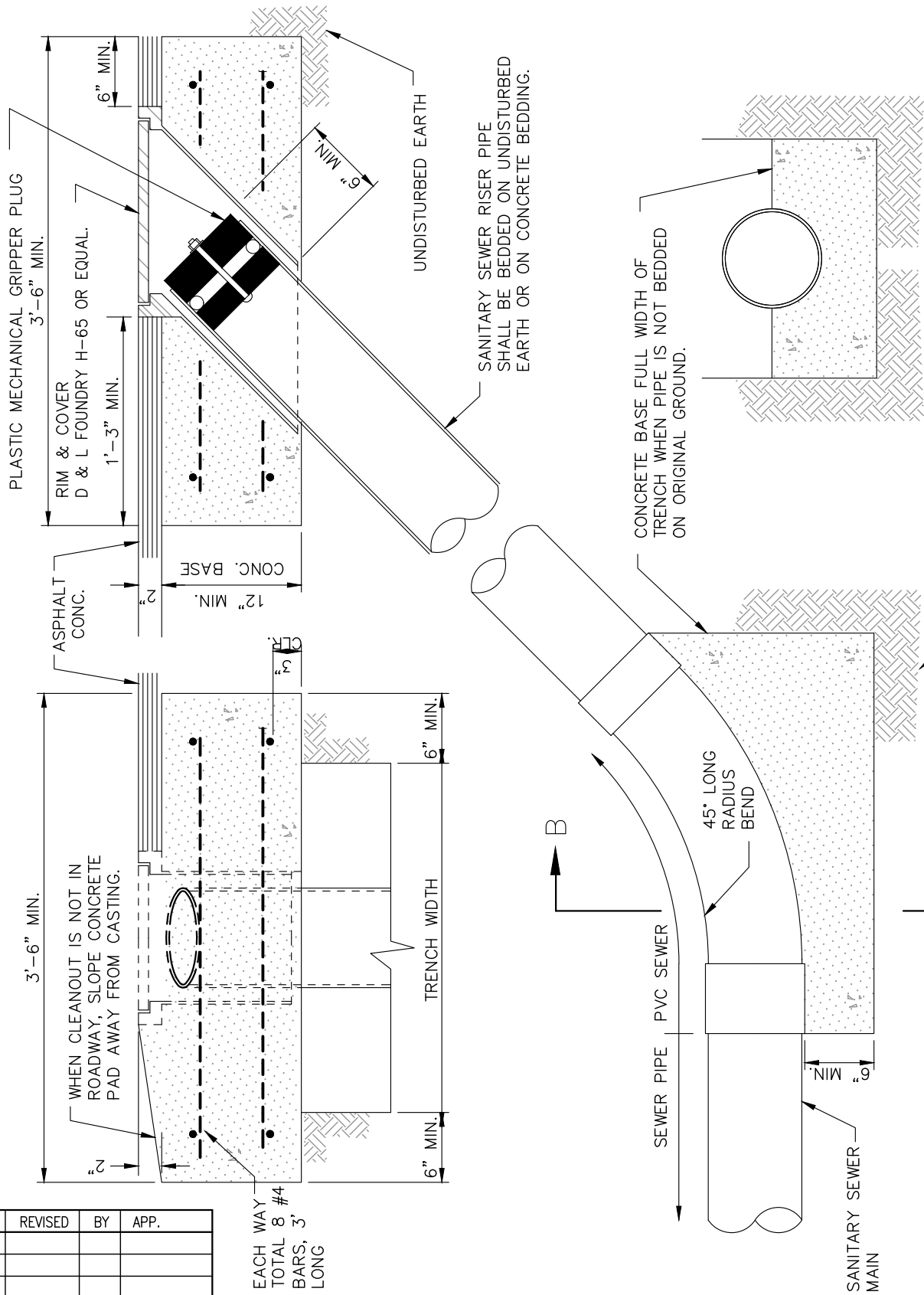
EASEMENT SERVICE
NTS

NOTE:

1. RECONSTRUCT SIDEWALK PER CITY STD. ST 1-5. INSTALL REBAR PER CITY STD. SS 1-2. LIMITS FOR CONCRETE RECONSTRUCTION TO BE NEAREST SCORE MARK OR AS DIRECTED BY CITY ENGINEER.

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DWN: CLG	4" SEWER LATERAL TWO-WAY CLEANOUT	SS 1-2
	CHK JT		PAGE 2 OF 2
DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	SCALE: NONE



SECTION B-B

NO.	REVISED	BY	APP.

EACH WAY
 TOTAL 8 #4
 BARS, 3'
 LONG



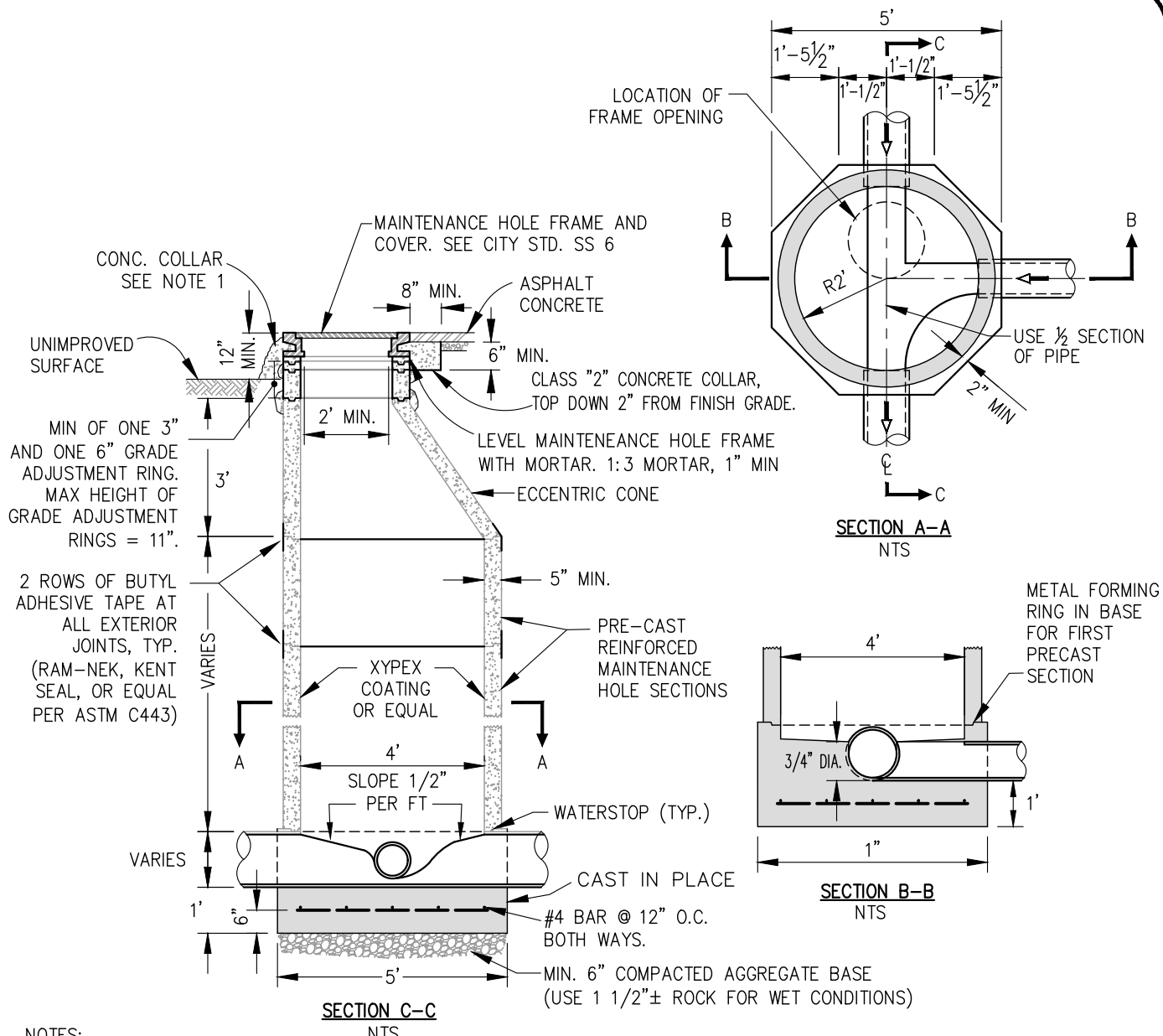
DWN: CLG
 CHK JT
 DEC 2021

CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT
45 DEGREE CLEANOUT (STREET)
 RUSS THOMPSON
 CITY ENGINEER *Russ Thompson*

STD DETAIL
SS 3
 PAGE 1 OF 1
 SCALE: NONE

DATE 12/07/21


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SS 4.dwg Layout Name: SS 4 Plot Date: Dec 07, 2021 at 13:00



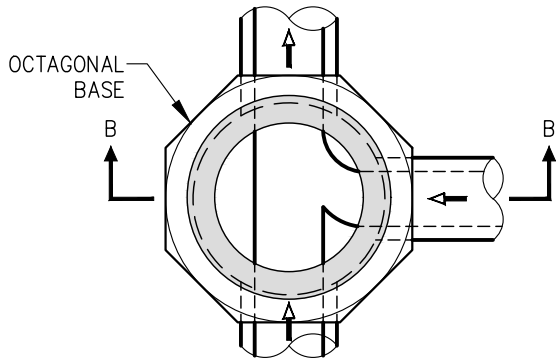
NOTES:

1. A RING OF MORTAR APPROXIMATELY 6" DEEP & EXTENDING PAST THE OUTER EDGE OF THE RING SHALL BE PLACED ALL AROUND & ON TOP OF THE BOTTOM FLANGE. THE MORTAR SHALL BE SMOOTHLY FINISHED & HAVE A SLIGHT SLOPE TO SHED WATER AWAY FROM THE FRAME (THIS CONDITION APPLIES IN NON-PAVEMENT AREAS ONLY. A STANDARD STREET PATCH SHALL BE USED IN PAVED AREAS).
2. RAM-NEK OR APPROVED EQUAL SHALL BE USED IN JOINTS. ALL JOINTS SHALL BE WATERTIGHT.
3. USE TYPE "A" MAINTENANCE HOLE FOR DEPTH OF COVER ON MAIN SEWER PIPE OVER 3.5 FT. SEE CITY STD. SS 5 FOR SHALLOWER DEPTHS.
4. WHEN MAIN IS 18" IN DIAMETER OR LARGER, ECCENTRIC CONE SHALL BE SET 90° FROM DIRECTION OF FLOW.
5. ASTM C923 RUBBER GASKETED WATERSTOP/MAINTENANCE HOLE ADAPTER REQUIRED FOR PIPE CONNECTIONS (A-LOK, KOR-N-SEAL, OR APPROVED EQUAL)
6. ALL MAINTENANCE HOLE INTERIORS TO BE COATED WITH A CEMENTITIOUS CRYSTALLINE WATERPROOFING (XYPEX OR APPROVED EQUAL)
7. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2 "MINOR CONCRETE" OF STATE SPECIFICATIONS, 3/4" AGGREGATE.

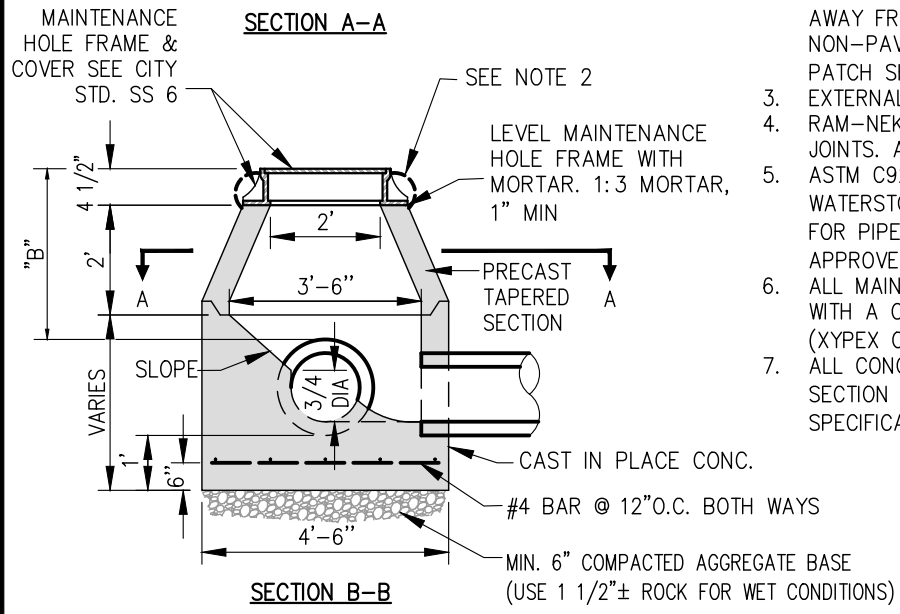
NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	PRE-CAST CONCRETE MAINTENANCE HOLE TYPE "A"	SS 4
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SS 5.dwg Layout Name: SS 5 Plot Date: Dec 07, 2021 at 13:01

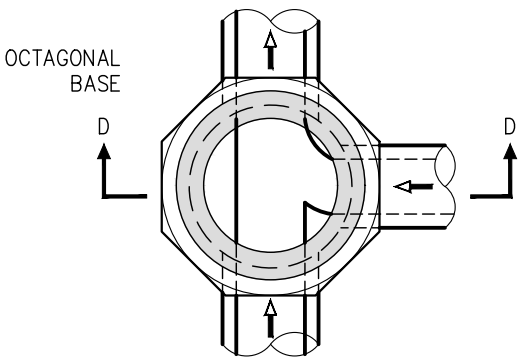


SECTION A-A

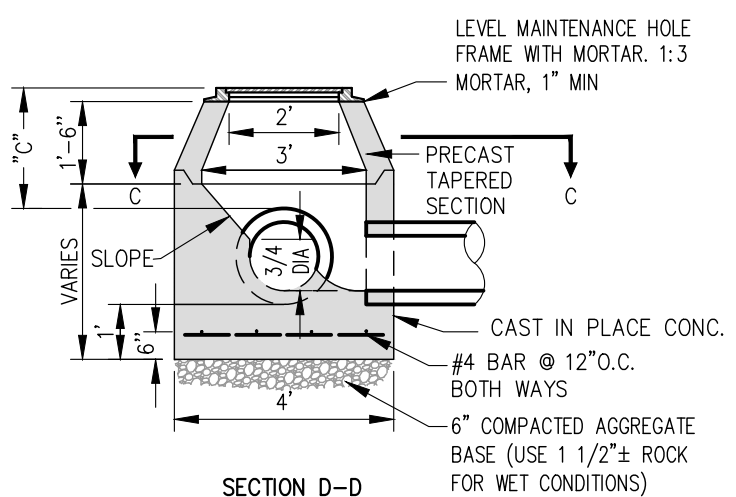


SECTION B-B

STANDARD MAINTENANCE HOLE TYPE B SHALL BE USED FOR VALUES OF "B" FROM 2'-6" TO 3'-6".



SECTION C-C




SECTION D-D

STANDARD MAINTENANCE HOLE TYPE C SHALL BE USED FOR VALUES OF "C" LESS THAN 2'-6".

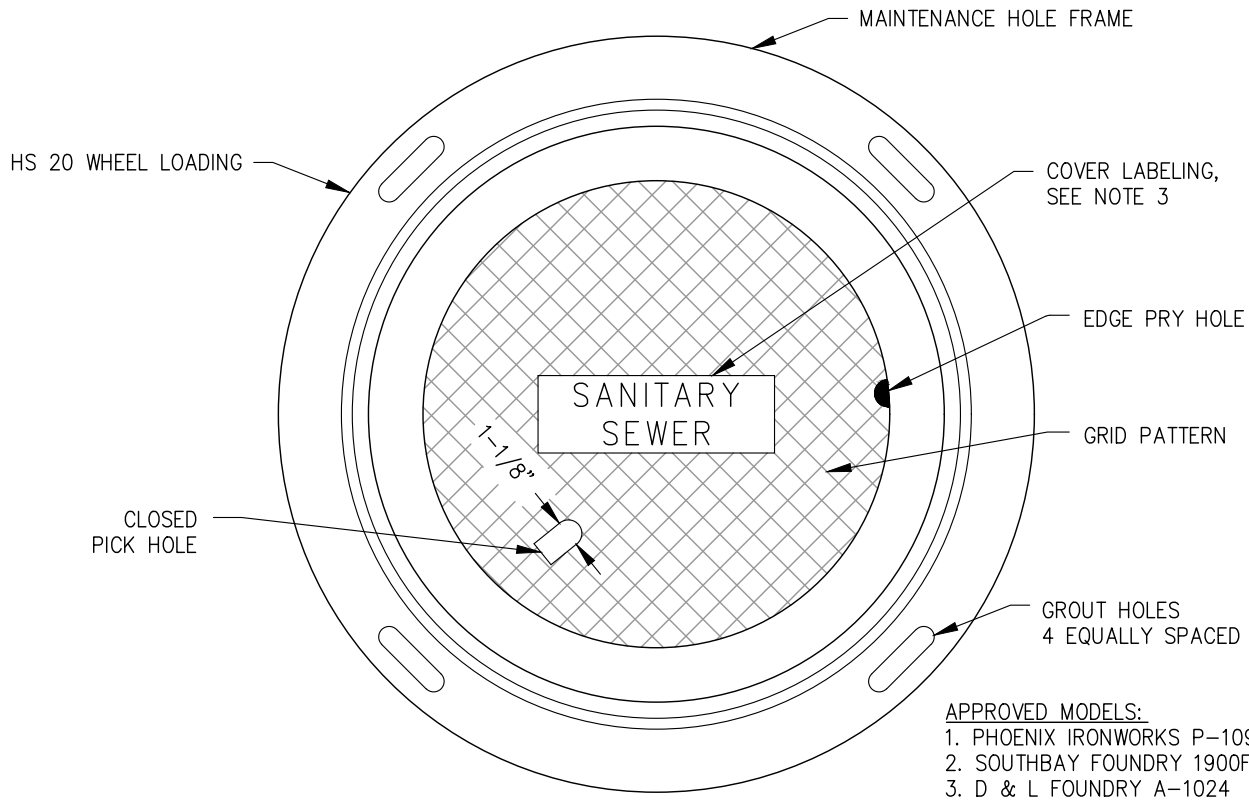
NOTES:

1. VALUES "B" AND "C" ARE DEPTHS FROM GROUND SURFACE TO TOP OF MAIN SEWER PIPE. SEWER LATERALS WITH COVER LESS THAN THAT OF MAIN SEWER MAY REQUIRE THE CHIPPING OUT OF A PORTION OF THE TAPERED SECTION OF THE MAINTENANCE HOLE TO ACCOMMODATE THE PIPE.
2. A RING OF MORTAR APPROXIMATELY 6" DEEP AND EXTENDING PAST THE OUTER EDGE OF THE RING SHALL BE PLACED ALL AROUND AND ON TOP OF THE BOTTOM FLANGE. THE MORTAR SHALL BE SMOOTHLY FINISHED AND HAVE A SLIGHT SLOPE TO SHED WATER AWAY FROM THE FRAME (THIS CONDITION APPLIES IN NON-PAVEMENT AREAS ONLY. A STANDARD STREET PATCH SHALL BE USED IN PAVED AREAS).
3. EXTERNAL BANDS SHALL BE APPLIED.
4. RAM-NEK OR APPROVED EQUAL SHALL BE USED IN JOINTS. ALL JOINTS SHALL BE WATERTIGHT.
5. ASTM C923 RUBBER GASKETED WATERSTOP/MAINTENANCE HOLE ADAPTER REQUIRED FOR PIPE CONNECTIONS (A-LOK, KOR-N-SEAL, OR APPROVED EQUAL).
6. ALL MAINTENANCE HOLE INTERIORS TO BE COATED WITH A CEMENTITIOUS CRYSTALLINE WATERPROOFING (XYPEX OR APPROVED EQUAL)
7. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2 "MINOR CONCRETE" OF STATE SPECIFICATIONS, 3/4" AGGREGATE.

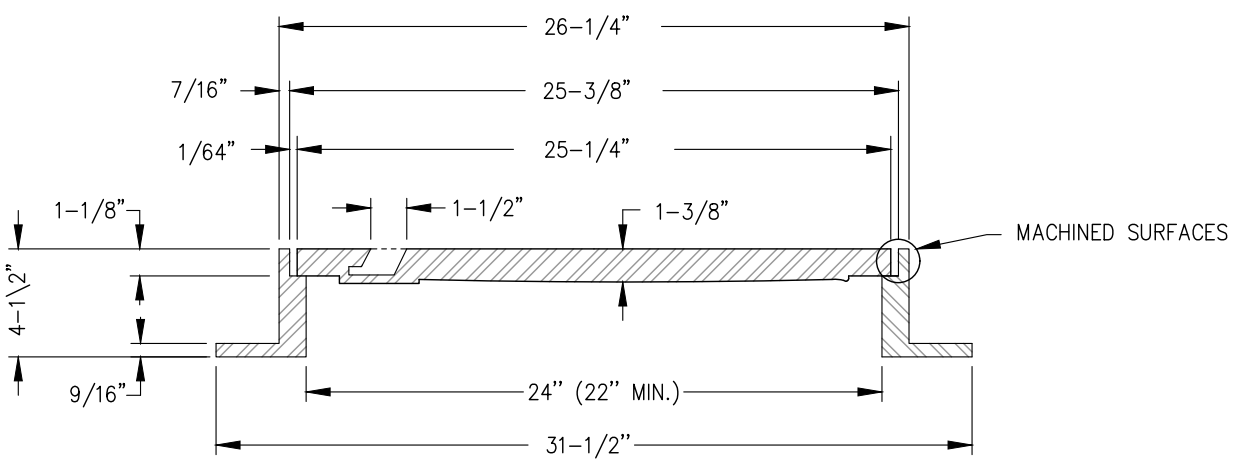
NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	MAINTENANCE HOLES TYPE "B" AND "C" SHALLOW DEPTH	SS 5
	DWN: CLG CHK: JT DEC 2021 RUSS THOMPSON CITY ENGINEER	DATE 12/07/21 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SS 6.dwg Layout Name: SS 6 Plot Date: Dec 07, 2021 at 13:03




- APPROVED MODELS:**
1. PHOENIX IRONWORKS P-1090
 2. SOUTHBAY FOUNDRY 1900FS
 3. D & L FOUNDRY A-1024



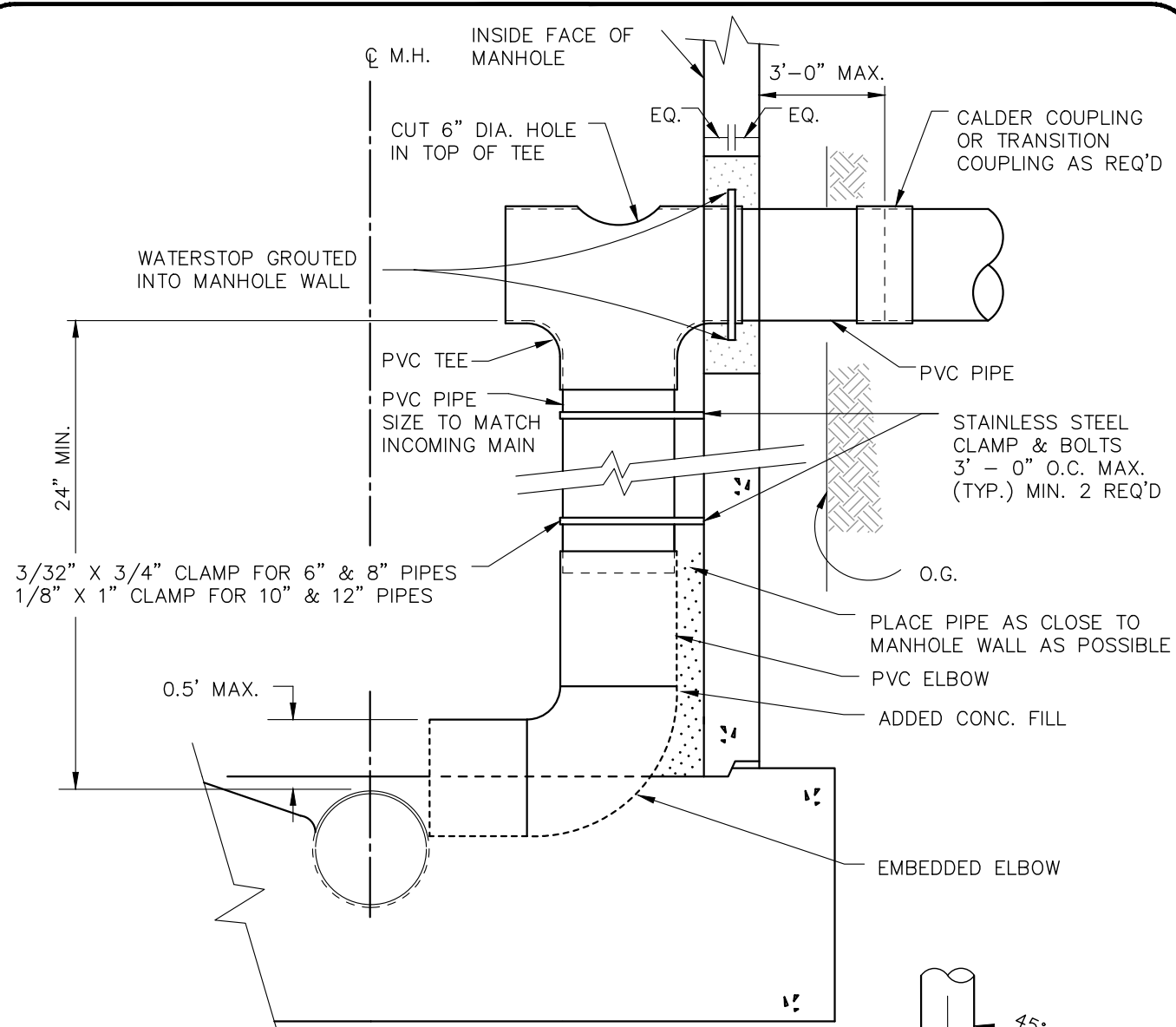
NOTES:

1. COVER AND FRAME SHALL BE MACHINED TO FIT ACCURATELY SO THAT COVER SHALL NOT ROCK OR RATTLE UNDER THE WHEEL OF TRAFFIC.
2. SOLID COVER WITHOUT HOLES (EXCEPT FOR PICK AND HOOK HOLES)
3. LID SHALL HAVE CAST-IN "SANITARY SEWER" LABEL, 1 INCH HIGH LETTERS, MINIMUM. (USE "STORM SEWER" LABEL WHERE APPROPRIATE.)
4. SOUTH BAY FOUNDRY SBF-1254 OR EQUAL.

NO.	REVISED	BY	APP.

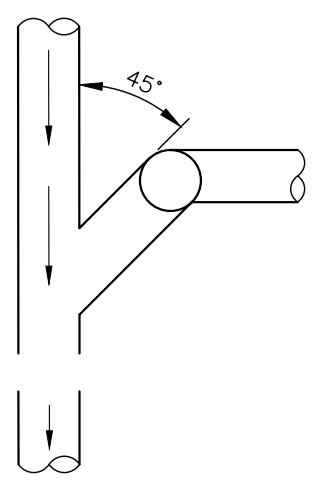
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	CHK JT	SANITARY SEWER MAINTENANCE HOLE COVER		SS 6
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 1 OF 1 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SS 7.dwg Layout Name: SS 7 Plot Date: Dec 07, 2021 at 13:04




NOTES

1. INSTALL WATERSTOP IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS AS SHOWN.
2. NEW MAINTENANCE HOLES CONSTRUCTED USING THIS STANDARD SHALL BE 48 INCHES IN DIAMETER, AND INSTALLED IN CONFORMANCE WITH STD PLAN SS 4.
3. ENCLOSE ELBOW IN CONCRETE. FORM SMOOTH CHANNEL TO MAINTENANCE HOLE FLOWLINE.
4. PVC PIPE AND FITTINGS TO BE SDR 35 OR SCH 40.

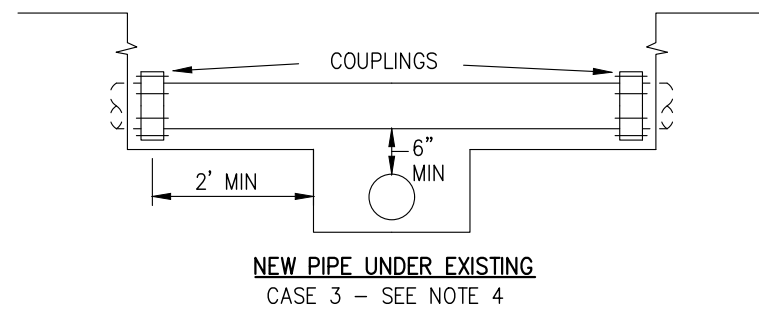
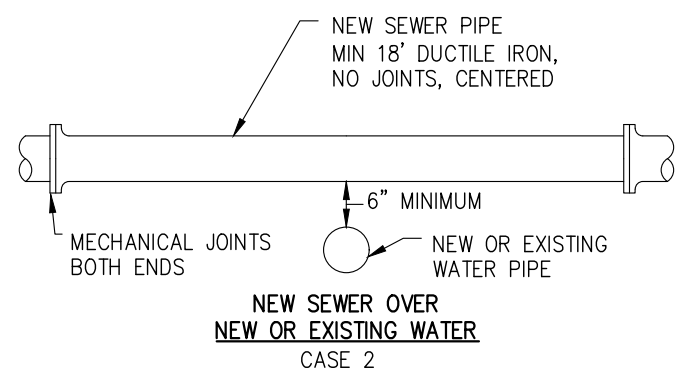
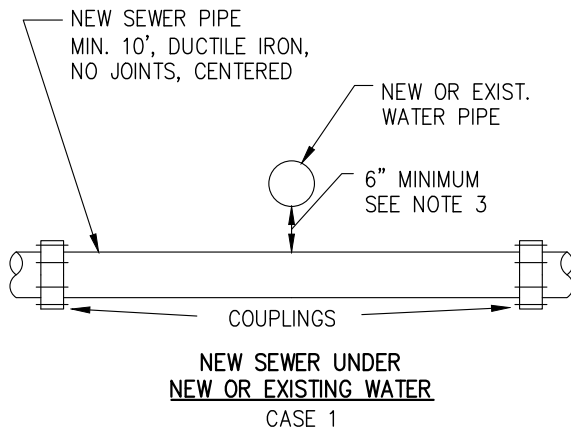


EMBEDDED ELBOW
ELBOW EMBEDDED AT 45° WITH SEWER FLOW

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	INSIDE DROP MAINTENANCE HOLE FOR 4" - 12" SEWER PIPES	SS 7
	DWN: CLG CHK JT DEC 2021	RUSSELL THOMPSON CITY ENGINEER <i>Russell Thompson</i> DATE 12/07/21



Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SS 8.dwg Layout Name: SS 8 Plot Date: Dec 07, 2021 at 13:06



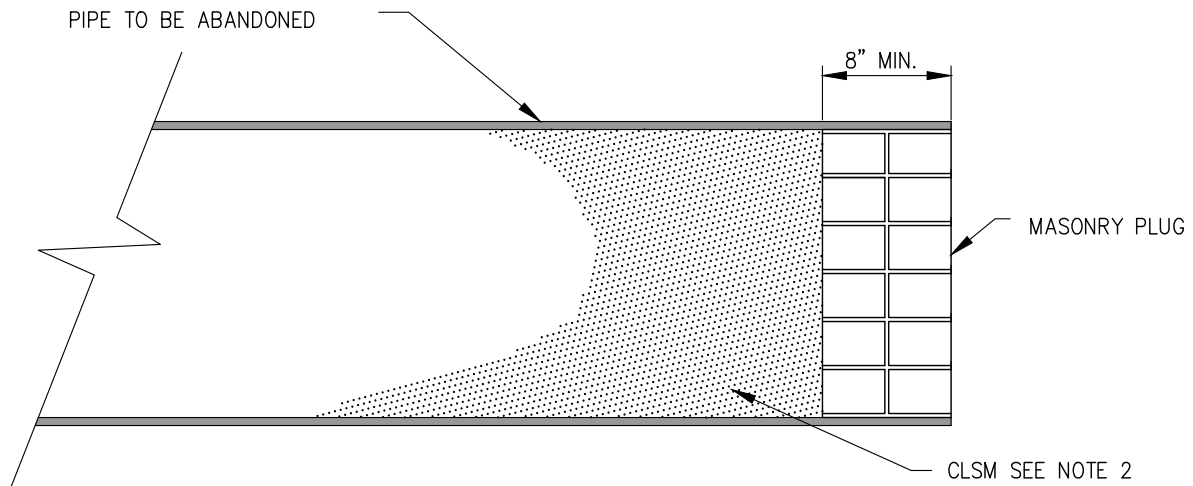
NOTES:

1. THIS STANDARD APPLIES TO PIPES UP TO AND INCLUDING 16" DIAMETER. ALL CROSSINGS OF LARGER DIAMETER SHALL BE AS APPROVED BY THE CITY ENGINEER.
2. ALL NEW DUCTILE IRON SHALL BE WRAPPED IN POLYETHYLENE PER CITY CONSTRUCTION SPECIFICATIONS.
3. WHERE SEWER CROSSES BELOW A WATER MAIN, WITH 1' OR MORE VERTICAL CLEARANCE, NO SPECIAL INSTALLATION IS REQUIRED.
4. "NEW PIPE UNDER EXISTING-CASE 3" SHALL BE USED WHEN THE EXISTING PIPE HAS A JOINT OVER OR WITHIN 2' OF THE NEW TRENCH.
5. ANY PIPE-PIPE CROSSING WITH LESS THAN 6" VERTICAL CLEARANCE SHALL NOT BE INSTALLED WITHOUT APPROVAL OF THE CITY ENGINEER.
6. SEE CITY APPROVED LIST FOR COUPLINGS.

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	PIPE CROSSING DETAILS	SS 8
DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER 	PAGE 1 OF 1 DATE 12/07/21 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SS 9.dwg Layout Name: SS 9 Plot Date: Dec 07, 2021 at 13:07



12" DIAMETER PIPE AND LARGER


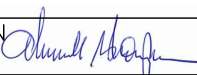


10" DIAMETER PIPE AND SMALLER

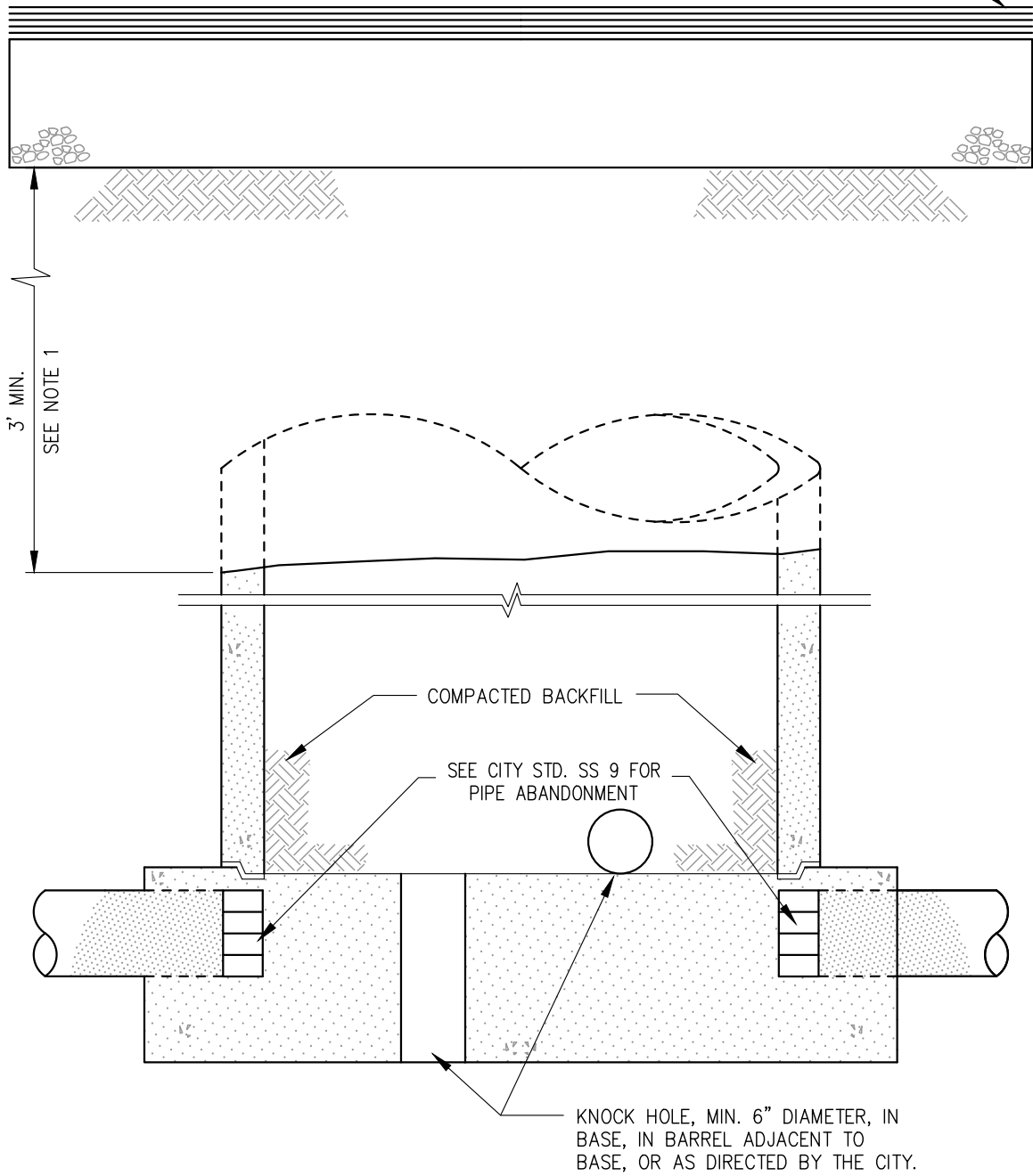
NOTES:

1. PIPE PLUGS SHALL BE INSTALLED TO THE SATISFACTION OF THE CITY ENGINEER.
2. ABANDONED PIPES, 12" AND LARGER, SHALL BE BROKEN INTO EVERY 50' AND SHALL BE FILLED COMPLETELY WITH CONTROLLED LOW-STRENGTH MATERIAL (CLSM).
3. SEWER LATERALS SHALL BE PLUGGED AND ABANDONED AT THE CONNECTION TO MAIN.

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	ABANDONED PIPE DETAIL	SS 9
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER 
DATE 12/07/21		

REPAIR OR REPLACE EX GROUND OR
PAVING PER CITY STD. ST 7-2


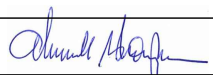


NOTES:

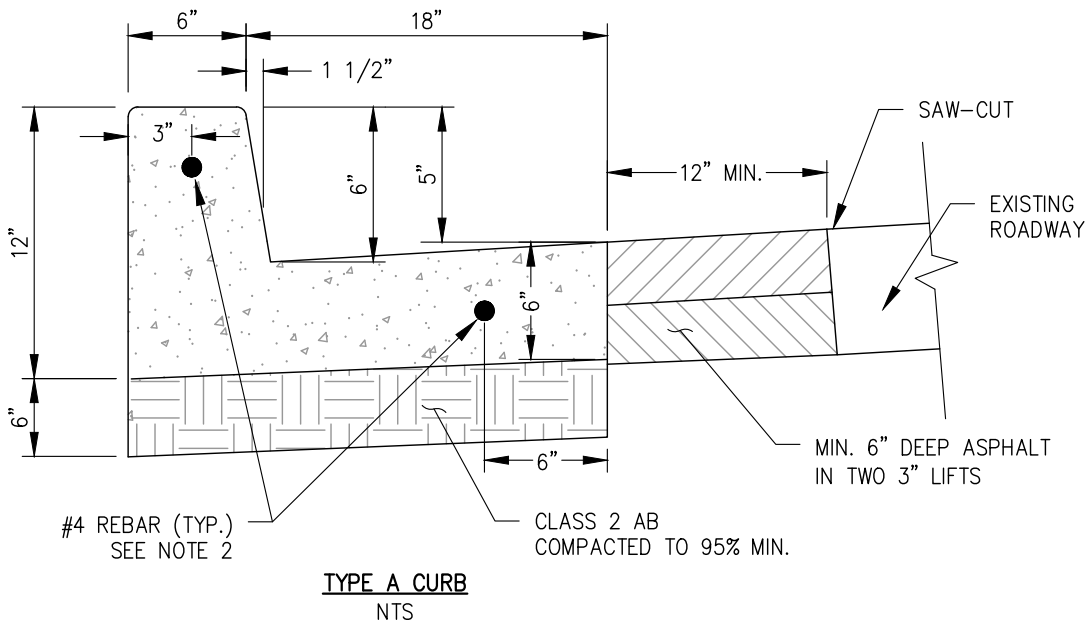
1. REMOVE FRAME, COVER, TAPER AND BARREL SECTIONS.
2. AFTER PLUGGING ALL PIPES IN MAINTENANCE HOLE, THE REMAINING PORTION OF THE BARREL SECTION AND ALL VOIDS CREATED BY THE REMOVAL OF THE UPPER PORTIONS OF THE MAINTENANCE HOLE, SHALL BE BACKFILLED AND COMPACTED TO 90% RELATIVE DENSITY. USE TRENCH BACKFILL OR PIPE BEDDING MATERIAL.

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs:
 Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SS 10.dwg Layout Name: SS 10 Plot Date: Dec 07, 2021 at 13:08

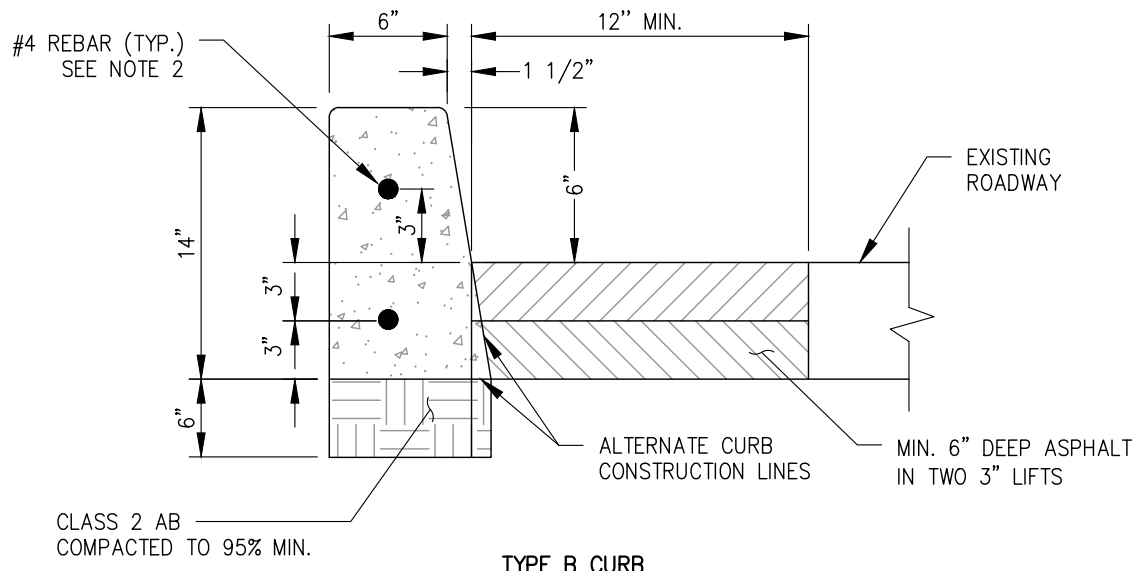
NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	ABANDONED MAINTENANCE HOLE DETAIL	SS 10
	DWN: CLG CHK JT DEC 2021	RUSSELL THOMPSON CITY ENGINEER 

DATE 12/07/21



TYPE A CURB
NTS




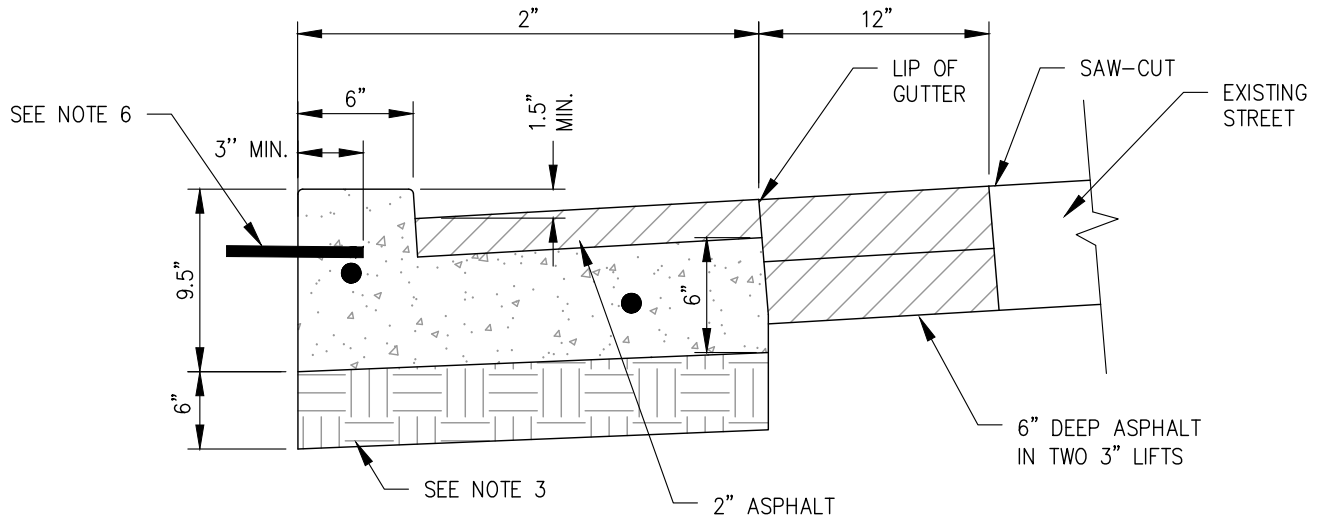
TYPE B CURB
NTS

NOTES:

1. PRIOR TO BEGINNING ANY WORK, AN ENCROACHMENT PERMIT SHALL BE OBTAINED.
2. CONTINUOUS #4 LONGITUDINAL REBAR, WITH 6" MINIMUM EMBEDMENT INTO EXISTING CURB & GUTTER. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.
3. 6" MINIMUM CLASS II AGGREGATE BASE, COMPACTED TO 95% RC.
4. 1/2" DEPTH DEEP JOINTS AT 20' MAX. 1/4" DEPTH CONTRACTION JOINTS AT 5' MAX AND SHALL BE SCORED TO MATCH ADJACENT SIDEWALK.
5. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2, "MINOR CONCRETE" OF STATE SPECIFICATIONS. 3/4" AGGREGATE WITH 1 1/2 POUNDS OF LAMP BLACK PER CUBIC YARD. FINISH TO BE UNIFORM MEDIUM BROOMED TEXTURE.
6. ASPHALT TO BE HOT MIX ASPHALT 1/2" MEDIUM.
7. NO PAVING AGAINST NEW CURB OR GUTTER FOR 7 DAYS.
8. LONGITUDINAL GUTTER FLOW LINE SHALL HAVE A MINIMUM SLOPE OF 0.6% (S=0.006).

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	TYPE A & B CONCRETE CURB AND GUTTER		ST 1-1
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 1 OF 6 SCALE: NONE




CURB AT DRIVEWAY
NTS

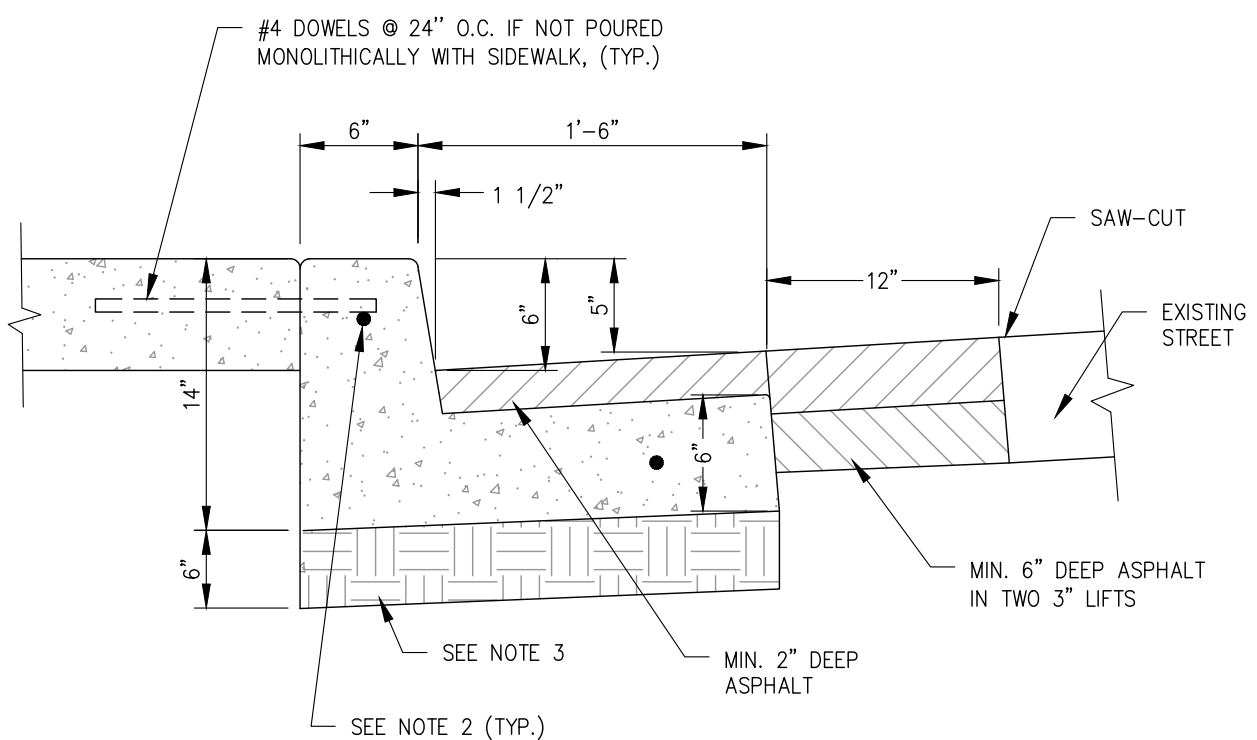
NOTES:

1. PRIOR TO BEGINNING ANY WORK, AN ENCROACHMENT PERMIT SHALL BE OBTAINED.
2. TO BE POURED MONOLITHIC WITH DRIVEWAY APPROACH.
3. 6" MINIMUM CLASS 2 A.B. COMPACTED TO 95%.
4. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2, "MINOR CONCRETE" OF STATE SPECIFICATIONS. 3/4" AGGREGATE WITH 1 1/2 POUNDS OF LAMP BLACK PER CUBIC YARD. FINISH TO BE UNIFORM MEDIUM BROOMED TEXTURE.
5. ASPHALT TO BE HOT MIX ASPHALT 1/2" MEDIUM.
6. 12" #4 REBAR DOWELS, 3" MINIMUM DEPTH INTO EXISTING CURB & GUTTER. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.
7. SEE DETAIL ST 3 AND ST 4 FOR DRIVEWAY APPROACH
8. SEE DETAIL ST 1-1 FOR CURB AND GUTTER

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CONCRETE CURB AND GUTTER AT DRIVEWAY APPROACH		ST 1-2
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 1-3.dwg Layout Name: ST 1-3 Plot Date: Dec 07, 2021 at 13:14



TYPE A CURB WITH ASPHALT
NTS

NOTES:

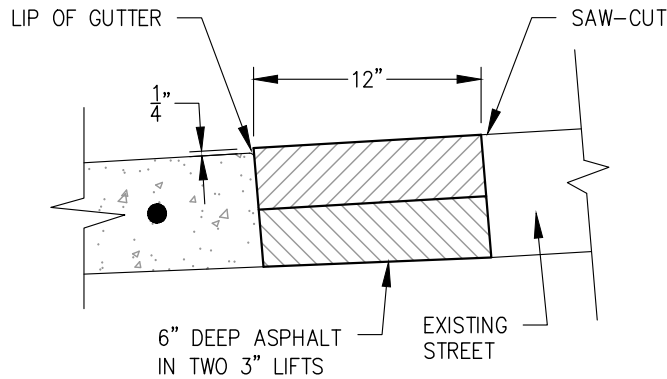
1. PRIOR TO BEGINNING ANY WORK, AN ENCROACHMENT PERMIT SHALL BE OBTAINED.
2. #4 REBAR CONTINUOUS DOWELS WITH 3" MINIMUM DEPTH INTO EXISTING CURB, 24" O.C. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.
3. 6" MINIMUM CLASS 2 A.B. COMPACTED TO 95%.
4. 1/2" DEEP CONTRACTION JOINTS AT 20', 1/4" DEEP CONTRACTION JOINTS AT 10'.
5. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2, "MINOR CONCRETE" OF STATE SPECIFICATIONS. 3/4" AGGREGATE WITH 1 1/2 POUNDS OF LAMP BLACK PER CUBIC YARD. FINISH TO BE UNIFORM MEDIUM BROOMED TEXTURE.
6. ASPHALT TO BE HOT MIX ASPHALT 1/2" MED.
7. CURB AND GUTTER AND SIDEWALK CONSTRUCTION SHALL CONFORM TO CITY STD. ST 1-1.
8. ALL SOFT OR SPONGY SUB-GRADE MATERIAL SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL AS REQUIRED BY THE CITY ENGINEER.
9. REINFORCING BARS SHALL BE MINIMUM GRADE 40 AND SHALL CONFORM TO SECTION 52, "REINFORCEMENT" OF THE CALTRANS STANDARD SPECIFICATIONS.
10. EPOXY USED FOR BONDING REINFORCING BARS TO EXISTING CONCRETE SHALL CONFORM TO THE REQUIREMENTS OF SECTION 95, EPOXY AND SECTION 95-2.03, "EPOXY RESIN ADHESIVE FOR BONDING NEW CONCRETE TO OLD CONCRETE" OF THE CALTRANS STANDARD SPECIFICATIONS.
11. CURB AND GUTTER SHALL BE SAWCUT AND REMOVED TO THE NEAREST CONTROL JOINT WHEN PRACTICAL AS DIRECTED BY THE CITY INSPECTOR.

NO.	REVISED	BY	APP.

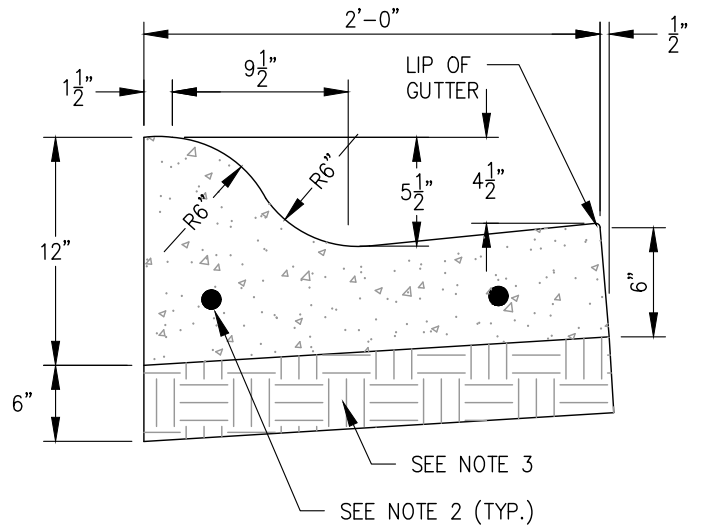
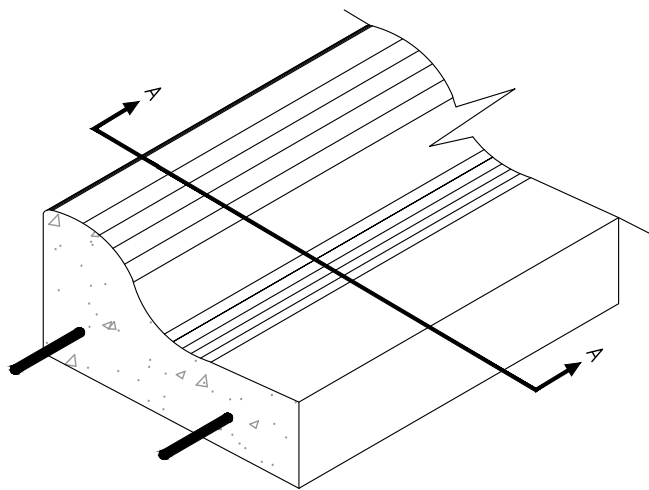


DWN: CLG CHK JT DEC 2021	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	
	TYPE A CONCRETE CURB AND GUTTER WITH ASPHALT	
RUSS THOMPSON CITY ENGINEER	<i>[Signature]</i> DATE 12/07/21	

STD DETAIL ST 1-3 PAGE 3 OF 6 SCALE: NONE



ASPHALT REPAIR AT GUTTER (TYP)


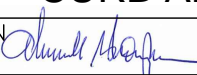


**TYPE E CURB
SECTION A-A**

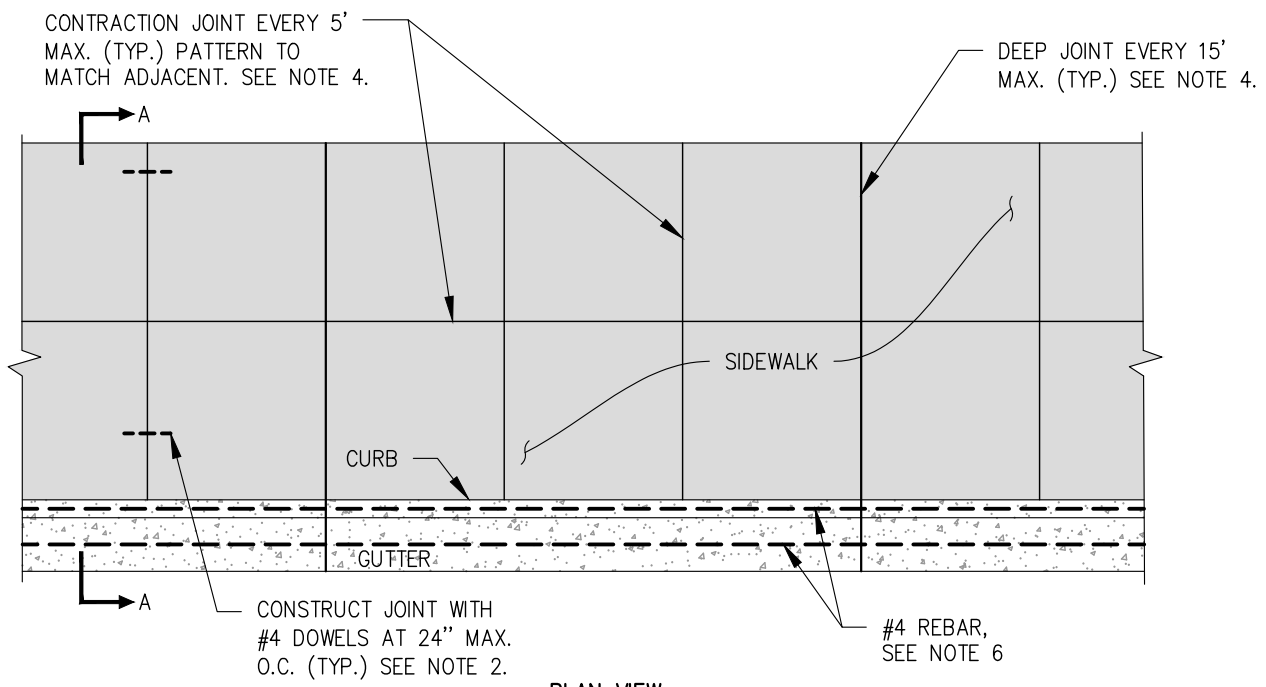
NOTES:

1. PRIOR TO BEGINNING ANY WORK, AN ENCROACHMENT PERMIT SHALL BE OBTAINED.
2. CONTINUOUS #4 LONGITUDINAL REBAR, WITH 6" MINIMUM EMBEDMENT INTO EXISTING CURB & GUTTER. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.
3. 6" MINIMUM CLASS 2 A.B. COMPACTED TO 95%.
4. 1/2" DEEP CONTRACTION JOINTS AT 20', 1/4" DEEP CONTRACTION JOINTS AT 10'.
5. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2, "MINOR CONCRETE" OF STATE SPECIFICATIONS. 3/4" AGGREGATE WITH 1 1/2 POUNDS OF LAMP BLACK PER CUBIC YARD. FINISH TO BE UNIFORM MEDIUM BROOMED TEXTURE.
6. ASPHALT TO BE HOT MIX ASPHALT 1/2" MEDIUM.

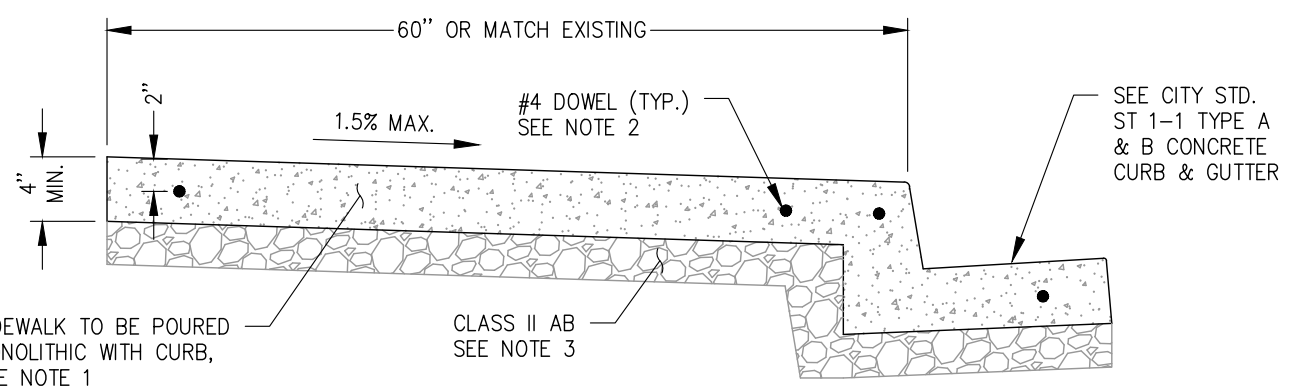
NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT		TYPE E CONCRETE CURB AND GUTTER
DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21	PAGE 4 OF 6 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 1-5.dwg
 Layout Name: ST 1-5 Plot Date: Dec 07, 2021 at 13:18



PLAN VIEW




SECTION A-A

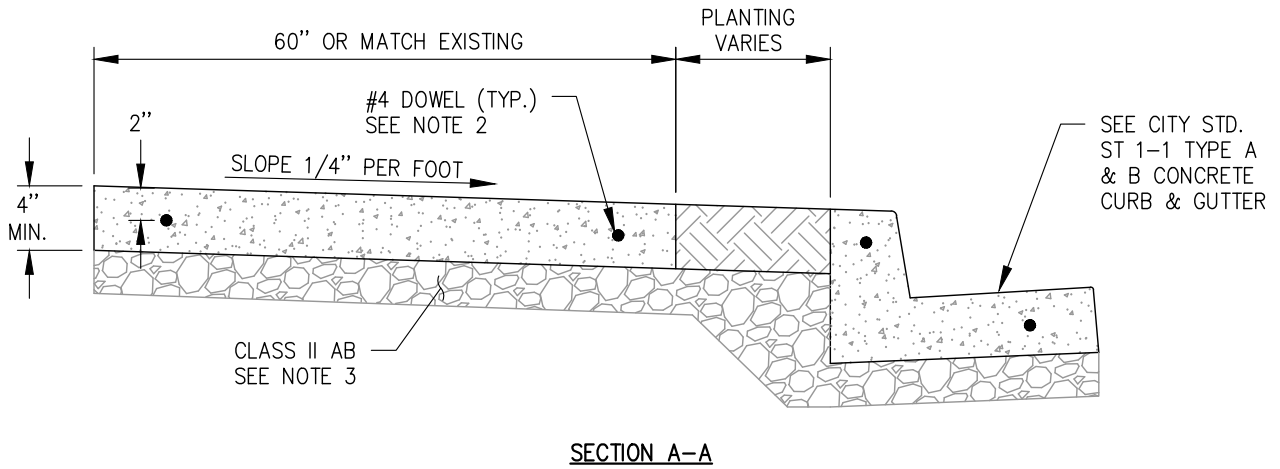
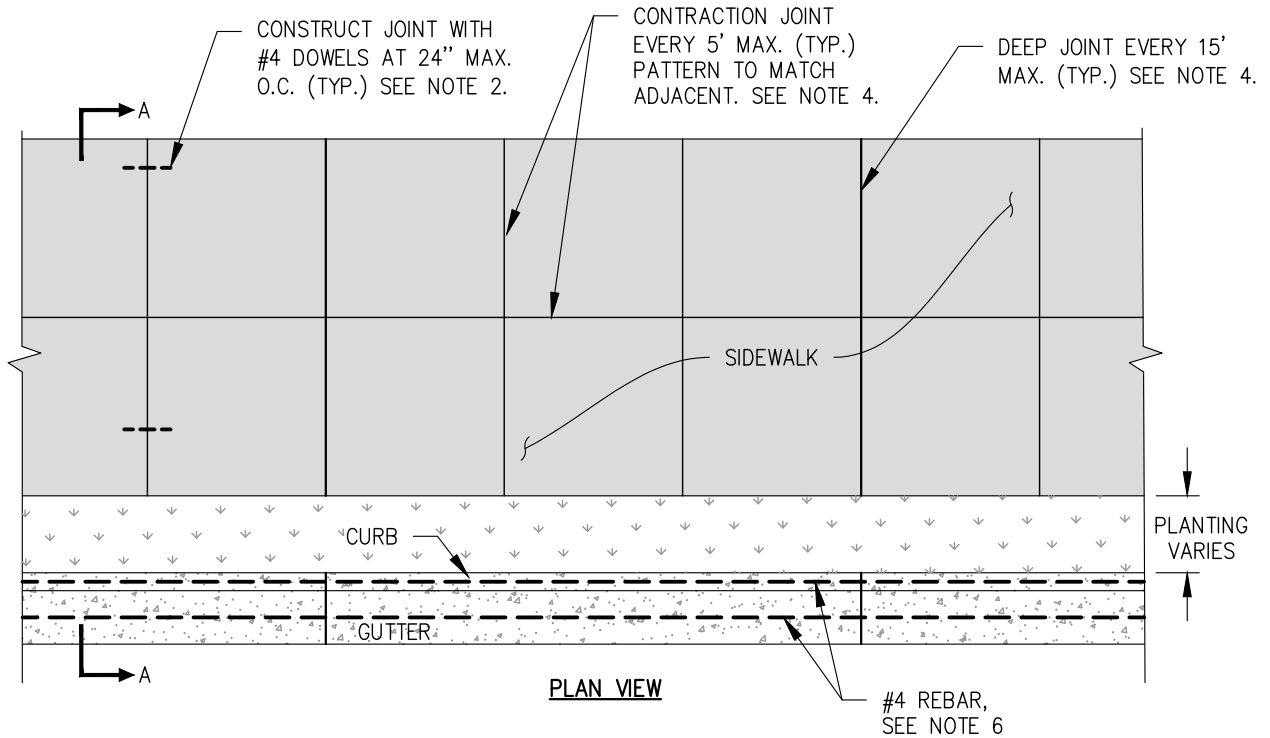
NOTES:

- CURB AND GUTTER TO BE CONSTRUCTED MONOLITHICALLY WITH SIDEWALK UNLESS APPROVED BY THE CITY ENGINEER. IF NOT POURED MONOLITHICALLY, DOWEL WITH #4 REBAR @ 24" O.C.
- 12" #4 REBAR DOWELS, 3" MINIMUM DEPTH INTO EXISTING OR NEW SIDEWALK. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.
- 4" MINIMUM CLASS II AB COMPACTED TO 95% MINIMUM.
- 1/2" DEPTH DEEP JOINTS AT 15' MAX, 1/4" DEPTH CONTRACTION JOINTS AT 5' MAX. AND SHALL BE SCORED TO MATCH EXISTING CONTIGUOUS SIDEWALK.
- ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 9-2, "MINOR CONCRETE" OF STATE SPECIFICATIONS, 3/4" AGGREGATE WITH 1 1/2 POUNDS OF LAMP BLACK PER CUBIC YARD.
- #4 REBAR CONTINUOUS IN NEW CURB AND GUTTER, DOWEL PER NOTE 2.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	CURB AND GUTTER WITH SIDEWALK		ST 1-5
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 5 OF 6
				SCALE: NONE


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 1-6.dwg Layout Name: ST 1-6 Plot Date: Dec 07, 2021 at 13:19



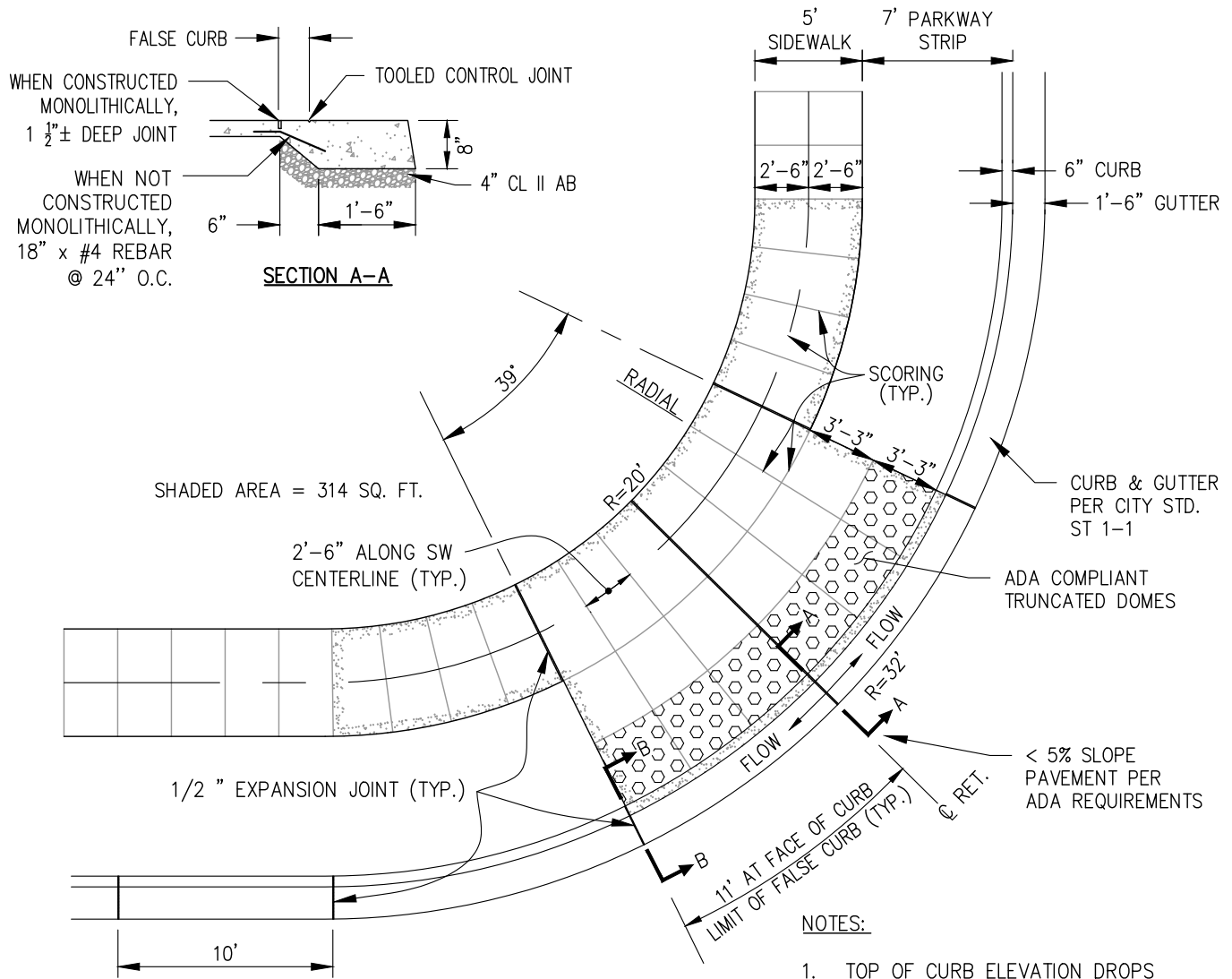
NOTES:

1. PRIOR TO BEGINNING ANY WORK, AN ENCROACHMENT PERMIT SHALL BE OBTAINED.
2. 12" #4 REBAR DOWELS, 3" MINIMUM DEPTH INTO EXISTING OR NEW SIDEWALK. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.
3. 6" MINIMUM CLASS II AB COMPACTED TO 95% MINIMUM.
4. 1/2" DEPTH DEEP CONTRACTION JOINTS AT 20', 1/4" DEPTH CONTRACTION JOINTS AT 10'. SIDEWALK SHALL BE SCORED TO MATCH CONTIGUOUS EXISTING SIDEWALK.
5. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2, "MINOR CONCRETE" OF STATE SPECIFICATIONS. 3/4" AGGREGATE WITH 1 1/2 POUNDS OF LAMP BLACK PER CUBIC YARD. FINISH TO BE UNIFORM MEDIUM BROOMED TEXTURE.
6. #4 REBAR CONTINUOUS IN NEW CURB AND GUTTER, DOWEL PER NOTE 2.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	VERTICAL CURB AND GUTTER PLANTER AND SIDEWALK		ST 1-6
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 6 OF 6 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 5.dwg Layout Name: ST 5 Plot Date: Dec 07, 2021 at 13:20



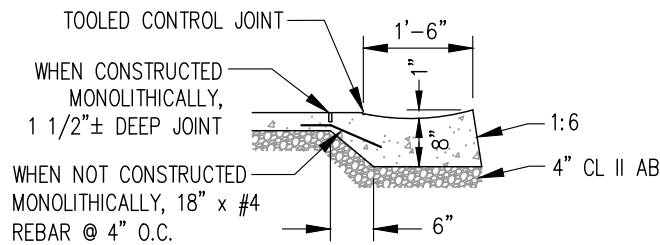
SHADED AREA = 314 SQ. FT.

1/2" EXPANSION JOINT (TYP.)

10'


NOTES:

1. TOP OF CURB ELEVATION DROPS DROPS 0.10' FROM A-A TO B-B .
2. FLOW LINE OF CURB ELEVATION DROPS 0.15' FROM A-A TO B- B .

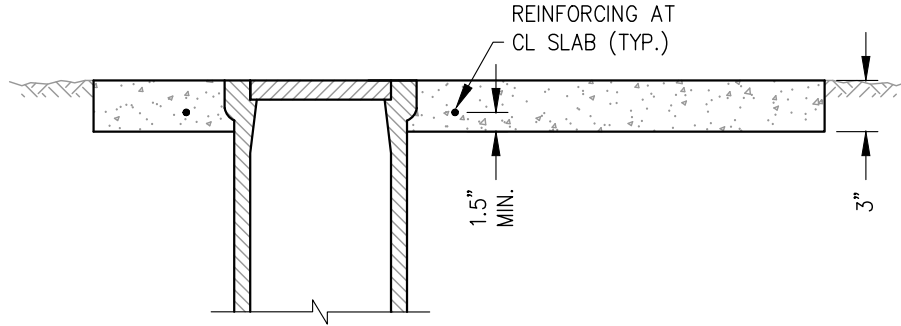


SECTION B-B

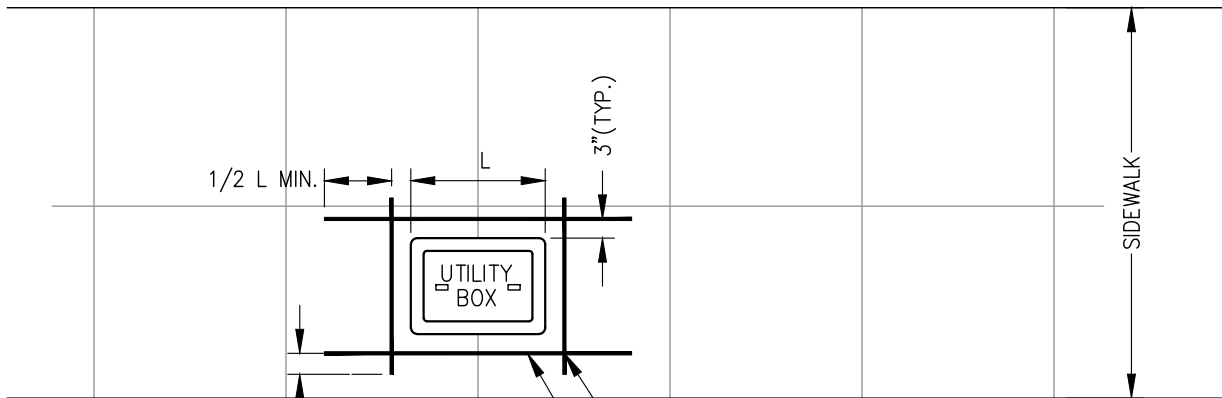
NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DWN: CLG	<h1>LARGE RADIUS FLUSH RETURN</h1>	
	CHK JT		
DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	ST 5 PAGE 1 OF 1 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 6.dwg Layout Name: ST 6 Plot Date: Dec 07, 2021 at 13:22


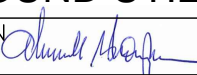


SECTION A-A

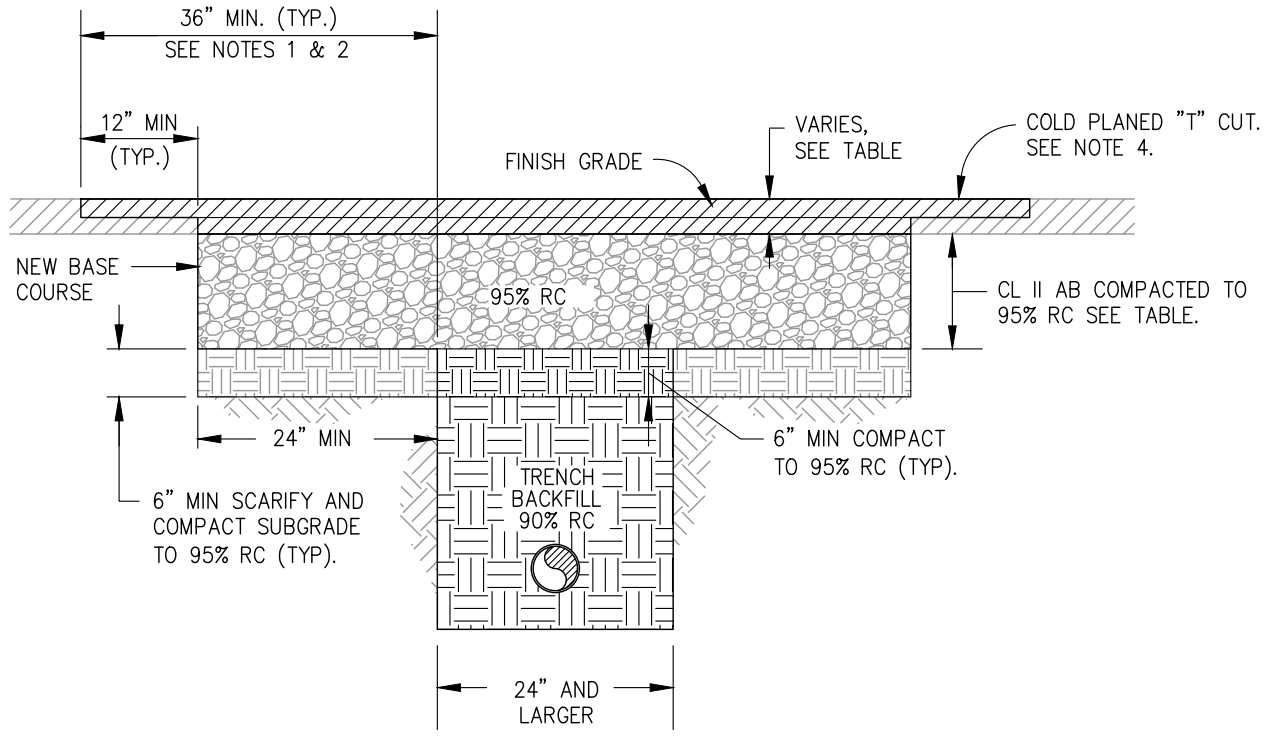


TIED WITH STEEL WIRE @ EACH CORNER
 #4 REINFORCING BAR ALL FOUR SIDES OF BOX WHERE BOX IS LARGER THAN 24" x 36".

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	SIDEWALK REINFORCING AROUND UTILITY BOX > 24"x36"		ST 6
DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21	PAGE 1 OF 1	
			SCALE: NONE	

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 7-1.dwg
 Layout Name: ST 7-1 Plot Date: Dec 07, 2021 at 13:30



TRENCHES OVER 24" INCHES WIDTH IN EXISTING PAVING

TRENCH PAVING TABLE (ASSUMES R = 10)		
STREET TYPE	MIN. HMA THICKNESS	MIN. CL II AB THICKNESS
RESIDENTIAL/LOCAL (TI = 5)	3"	9"
COLLECTOR (TI = 7)	4"	14"
ARTERIAL (TI = 9)	6"	18"

NOTES:

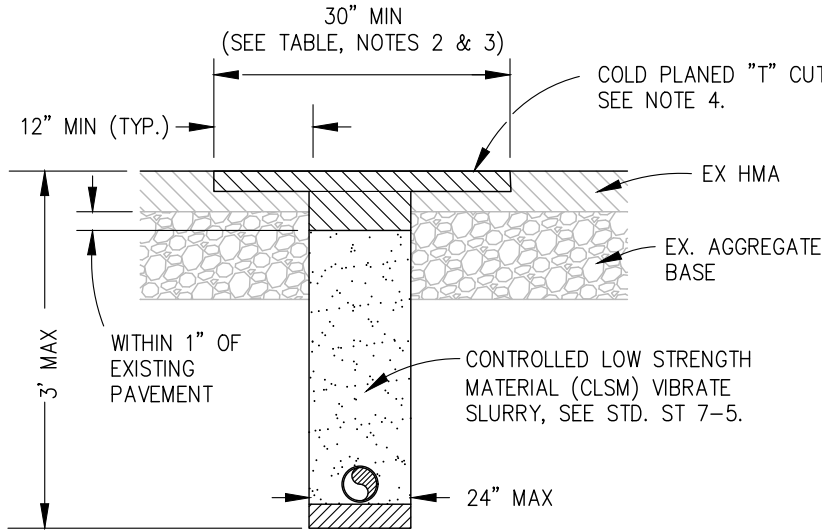
1. NEATLY CUT PAVEMENT AFTER TRENCH IS BACKFILLED TO SUBGRADE. ADDITIONAL PAVEMENT REMOVAL: REMOVE ADDITIONAL PAVEMENT TO A PAINTED LANE STRIPE, A LIP OF GUTTER, EXISTING PAVEMENT PATCH, OR AN EDGE OF THE PAVEMENT IF SUCH STREET FEATURE IS WITHIN 3 FEET OF THE FINAL SAW CUT.
2. FULL TACK COAT COVERAGE ON ALL VERTICAL AND EXISTING HMA SURFACES.
3. RELATIVE COMPACTION IS DESIGNATED AS RC.
4. 1-1/2" SURFACE COURSE IF EXISTING HMA IS GREATER THAN OR EQUAL TO 3" THICK. IF HMA IS LESS THAN 3" THICK, REPLACE WITH FULL DEPTH HMA.
5. SECTIONS MAY BE REDUCED BASED ON ACTUAL R-VALUES.

NO.	REVISED	BY	APP.

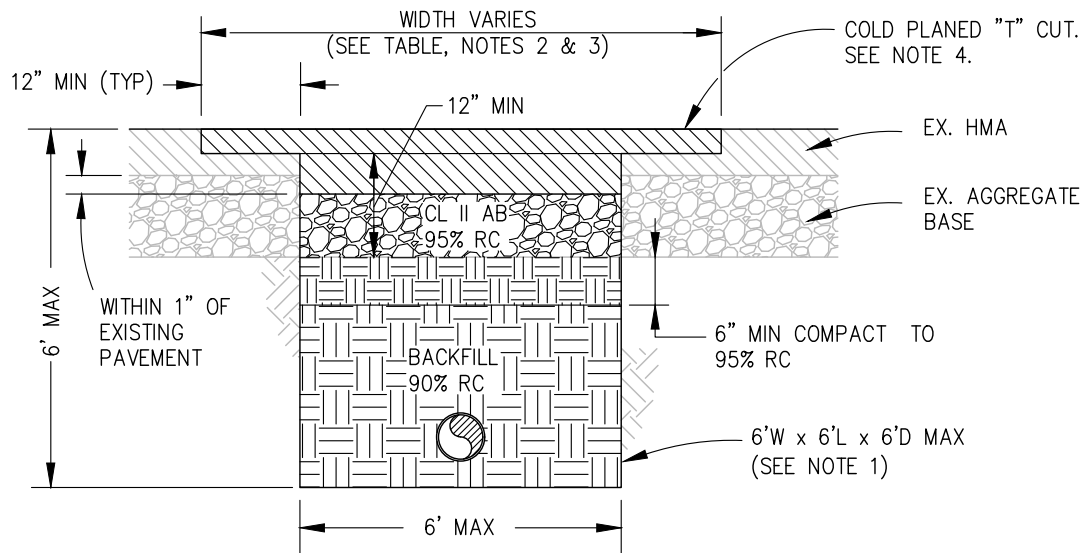
	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT TRENCH DETAIL RESURFACING	STD DETAIL
	CHK JT		ST 7-1
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21

NOTES:

1. IF ANY DIMENSION IS GREATER THAN 6' OR THE HOLE IS NOT BACKFILLED ON THE SAME DAY OF EXCAVATION, USE STD, 7-1 TRENCH RESURFACING DETAIL.
2. NEATLY CUT PAVEMENT AFTER TRENCH IS BACKFILLED TO SUBGRADE. ADDITIONAL PAVEMENT REMOVAL: REMOVE ADDITIONAL PAVEMENT TO A PAINTED LANE STRIPE, A LIP OF GUTTER, EXISTING PAVEMENT PATCH, OR AN EDGE OF THE PAVEMENT IF SUCH STREET FEATURE IS WITHIN 3 FEET OF THE FINAL SAW CUT.
3. FULL TACK COAT COVERAGE ON ALL VERTICAL AND EXISTING HOT MIX ASPHALT (HMA) SURFACES.
4. PLACE 1-1/2" SURFACE COURSE IF EXISTING HMA IS GREATER THAN OR EQUAL TO 3" THICK. IF HMA IS LESS THAN 3" THICK, REPLACE WITH FULL DEPTH HMA.




TRENCHES UNDER 24" WIDTH IN EXISTING PAVING



SMALL HOLES (ONLY USED IN EXISTING PAVED AREAS)

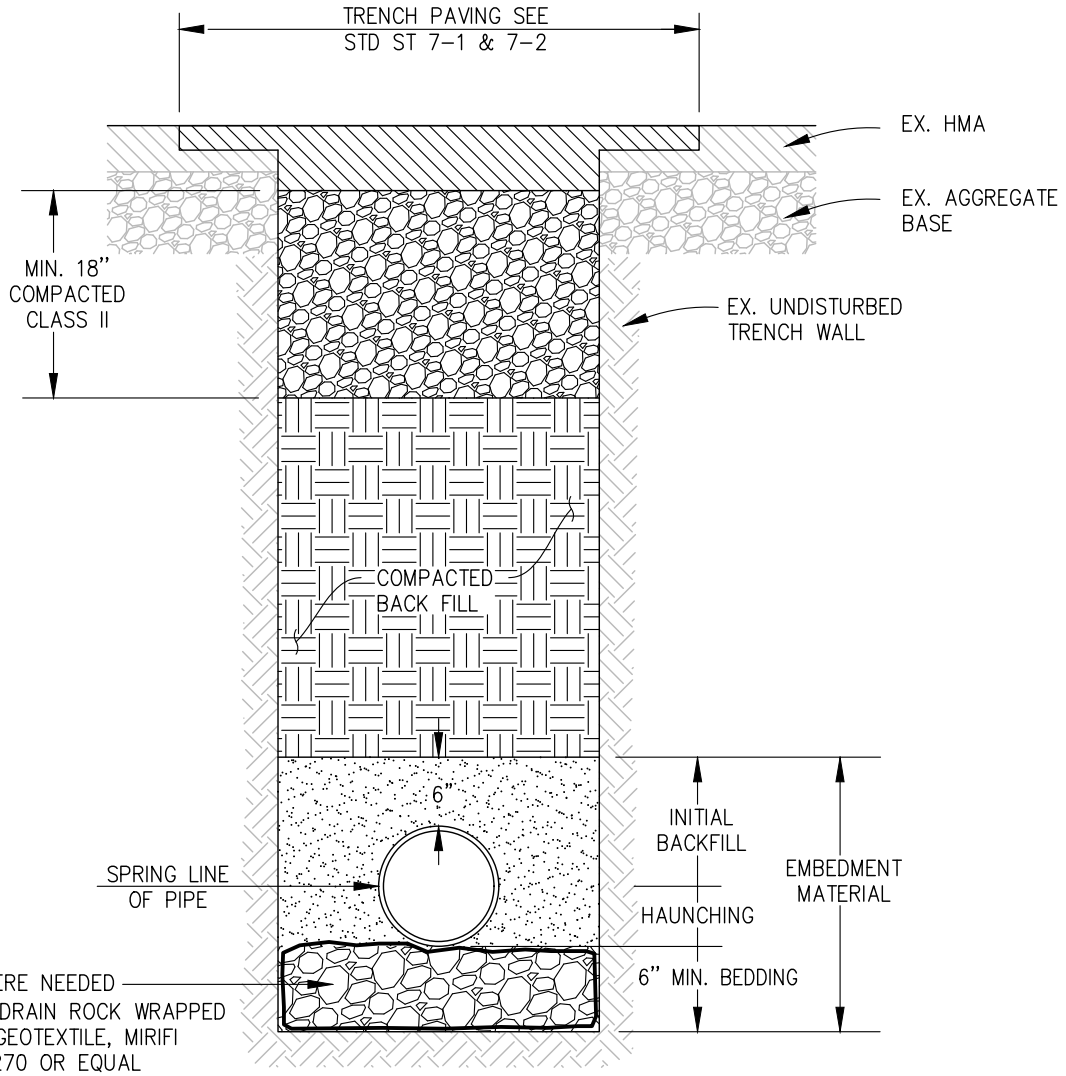
TRENCH PAVING TABLE	
STREET TYPE	MIN. HMA THICKNESS
RESIDENTIAL/LOCAL	3"
COLLECTOR	4"
ARTERIAL	6"

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	TRENCH DETAIL NARROW TRENCH AND SMALL HOLES		ST 7-2
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 2 OF 5 SCALE: NONE

Images: Alameda.png; RST Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 7-2.dwg
 Layout Name: ST 7-2 Plot Date: Dec 07, 2021 at 13:31


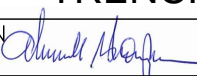
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 Layout Name: ST 7-3 Plot Date: Dec 07, 2021 at 13:32



NOTES:

1. EMBEDMENT MATERIAL MUST BE CLASS I.
2. EMBEDMENT MATERIAL SHALL BE COMPACTED TO A MINIMUM 90% STANDARD PROCTOR DENSITY FOR CLASS I MATERIAL.
3. STANDARD BEDDING SHALL BE UTILIZED FOR ALL CASES WHERE TRENCH BOTTOMS ARE UNSTABLE DUE TO SOIL TYPE OR MOISTURE CONDITIONS.
4. VOIDS AND HAUNCH AREA ARE TO BE HAND FILLED.
5. UNDER ENGINEERS APPROVAL, COMPACTED BACK FILL MAY BE NATIVE MATERIAL.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	TRENCH DETAIL TRENCH BACKFILL	ST 7-3
	DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21

MATERIAL SPECIFICATIONS

DRAIN ROCK MAY BE USED AS BEDDING UNDER PIPE FOR SLOPES LESS THAN 8%. DRAIN ROCK SHALL BE 100% CRUSHED AND SHALL CONFORM TO THE FOLLOWING GRADING:

1-1/2"	1"	1/2"	#4
100	95-100	0-30	0-4

CUT OFF WALLS SHALL BE PROVIDED WHEN SHOWN ON THE PLANS OR REQUIRED BY THE CITY ENGINEER.

PIPE BEDDING FOR SLOPES LESS THAN OR EQUAL TO 8% SHALL HAVE A MINIMUM SAND EQUIVALENT VALUE OF 30 AND SHALL CONFORM TO THE FOLLOWING GRADING:

1"	3/4"	3/8"	#4	#200
100	90-100	65-100	30-100	0-15

PIPE BEDDING FOR SLOPES GREATER THAN 8% SHALL HAVE A MINIMUM SAND EQUIVALENT OF 30 AND SHALL CONFORM TO THE FOLLOWING GRADING:

1"	3/4"	3/8"	#4	#30	#200
100	90-100	65-100	30-100	10-100	0-15

TRENCH BACKFILL SHALL CONFORM TO THE FOLLOWING GRADING AND HAVE A MINIMUM SAND EQUIVALENT VALUE OF 25.

3"	#4	#30
100	40 -100	10-100

WHEN SAND BEDDING IS PROPOSED FOR UTILITY CONDUIT INSTALLATION, THE MATERIAL SPECIFICATIONS SHALL BE PROVIDED TO THE ENGINEER FOR REVIEW 5 WORKING DAYS BEFORE CONSTRUCTION.

AGGREGATE BASE SHALL CONFORM TO THE REQUIREMENTS OF SECTION 26 OF THE STATE STANDARD SPECIFICATIONS. HOT MIX ASPHALT SHALL CONFORM TO THE REQUIREMENTS OF SECTION 39 OF THE STATE STANDARD SPECIFICATIONS.

COMPACTION REQUIREMENTS

AS SHOWN ON STD. ST 7-1, ST 7-2, ST 7-3 AND IN THE FOLLOWING MODIFICATIONS


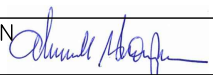
DRAIN ROCK SHALL BE CONSOLIDATED WITH A SURFACE VIBRATOR.

PIPE BEDDING MATERIAL USED TO GRADE THE TRENCH SHALL BE CONSOLIDATED WITH A SURFACE VIBRATOR WHEN IT IS PLACED OVER DRAIN ROCK OR WHEN DEPTH IS GREATER THAN 6 INCHES.

TRENCH BACKFILL MAY NOT BE COMPACTED BY JETTING.

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 7-4.dwg
 Layout Name: ST 7-4 Plot Date: Dec 07, 2021 at 13:33

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	TRENCH DETAIL	
	DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21

CONTROLLED LOW STRENGTH MATERIAL (CLSM)

CLSM SHALL BE A MIXTURE OF PORTLAND CEMENT, SAND AND 1" MAXIMUM COARSE AGGREGATE, AIR ENTRAINING AGENT AND WATER, BATCHED BY A READY-MIXED CONCRETE PLANT AND DELIVERED TO THE JOBSITE BY MEANS OF TRANSIT MIXING TRUCKS. CONTROL DENSITY FILL MAY ALSO CONTAIN CLASS F POZZOLON (FLY ASH). CLSM SHALL BE FREE OF ASPHALTIC MATERIAL.

MATERIALS

CEMENT SHALL MEET THE STANDARDS AS SET FORTH IN ASTM C-150, TYPE II CEMENT.

FLY ASH SHALL MEET THE STANDARDS AS SET FORTH IN ASTM C-618, FOR CLASS F POZZOLANS. THE FLY ASH SHALL NOT INHIBIT THE ENTRAINMENT OF AIR.

AGGREGATE SIZE 1" MAX.
SAND EQUIVALENT 31 MM.

MIX PROPORTIONS

THE MIX PROPORTIONS SHALL BE DETERMINED BY THE PRODUCER OF THE CLSM TO PRODUCE A FLOWABLE FILL MIXTURE WHICH WILL NOT SEGREGATE. EACH YARD SHALL CONTAIN NOT LESS THAT 50 POUNDS OF PORTLAND CEMENT AND NOT LESS THAN A TOTAL OF 100 POUNDS OF CEMENTITIOUS MATERIAL, THE CONTRACTOR SHALL SUPPLY A MIX DESIGN TWO WEEKS PRIOR TO ANY USE OF CLSM.

MIXTURE PROPERTIES

COMPRESSIVE STRENGTH 75- 200 PSI @ 28 DAYS
SLUMP 3 - 9 INCHES

THE CONSISTENCY OF CLSM SHALL BE SUCH THAT ALL TRENCH VOIDS ARE FILLED WITH MINIMUM RODDING OR VIBRATING BUT NOT SO WET AS TO CAUSE EXCESSIVE SHRINKAGE.


PAVING

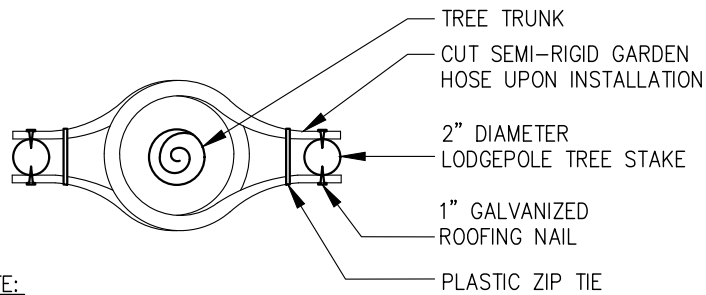
PERMANENT PAVEMENT MAY BE PLACED DIRECTLY UPON THE CLSM AS SOON AS IT HAS CONSOLIDATED FOR THE SURFACE TO WITHSTAND THE PROCESS OF PAVING WITHOUT DISPLACEMENT. THE SURFACE OF THE CLSM SHALL BE FIRM AND UNYIELDING. ANY VISIBLE MOVEMENT VERTICALLY OR HORIZONTALLY OF THE CLSM UNDER THE ACTION OF CONSTRUCTION EQUIPMENT OR OTHER MAXIMUM LEGAL AXLE LOADS SHALL BE CONSIDERED AS EVIDENCE THAT THE CLSM DOES NOT MEET THIS REQUIREMENT. THE CONTRACTOR SHALL PROVIDE TRENCH PLATES TO ALLOW TRAFFIC FLOW FOR ALL LOCATIONS UNTIL CLSM IS READY TO BE PAVED.

Layout Name: ST 7-5 Plot Date: Dec 07, 2021 at 13:34

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 7-5.dwg

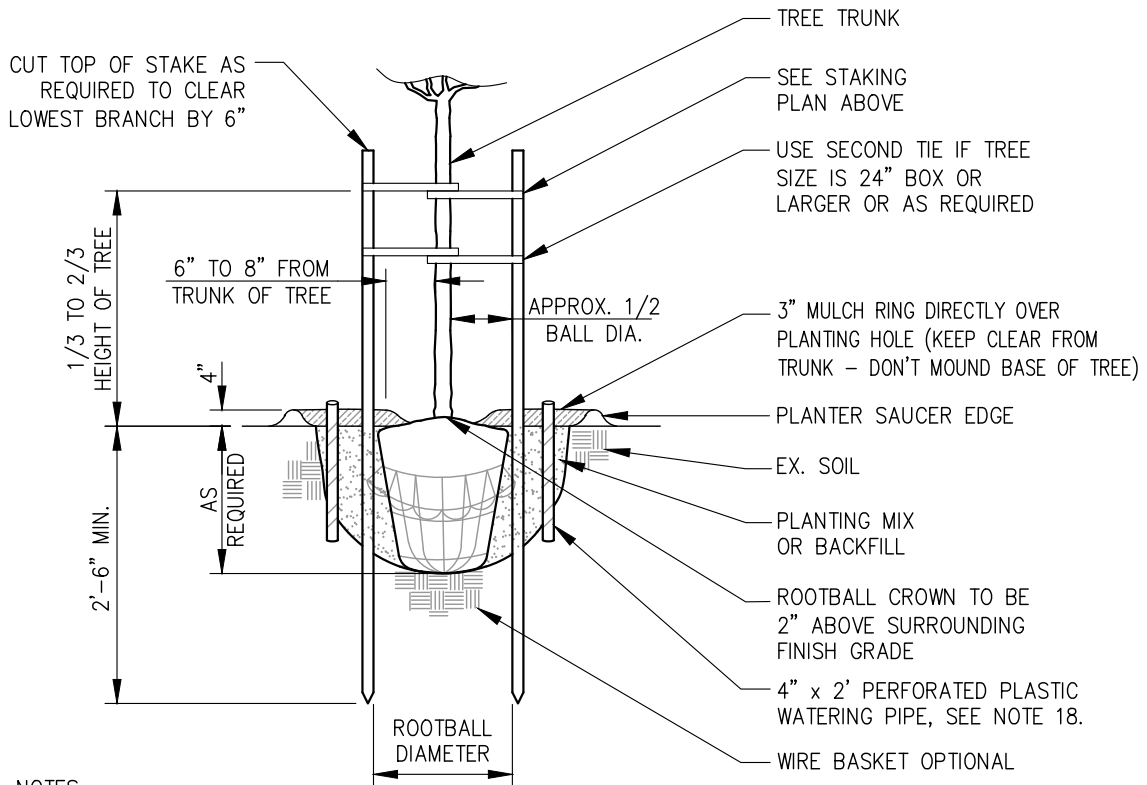
NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	TRENCH DETAIL	ST 7-5
	DEC 2021	CONTROLLED LOW STRENGTH MATERIAL (CLSM)	PAGE 5 OF 5
	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	SCALE: NONE



NOTE:
 TREE STAKES FOR ALL STREET TREES SHALL BE PARALLEL TO STREET

TREE STAKING PLAN




NOTES:

1. CUT AND REMOVE TOP 1/2 OF WIRE BASKET, TURN DOWN TOP 1/2 OF BURLAP; REMOVE ALL CORD & TWINE FROM BASE OF TRUNK; IF NON-DEGRADABLE WRAP IS USED, REMOVE TOTALLY.
2. LOOSEN EXTERIOR OF THE ROOT BALL AND THE ROOT MAT AT THE BOTTOM OF THE BALL, CUT ALL MAJOR CIRCLING ROOTS.

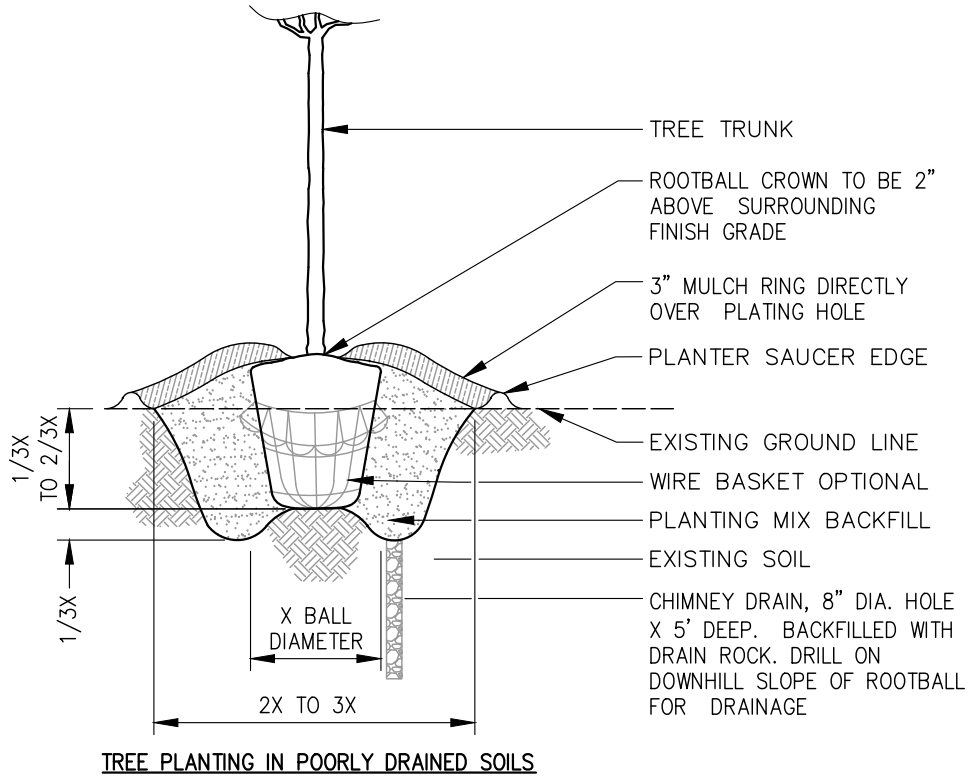
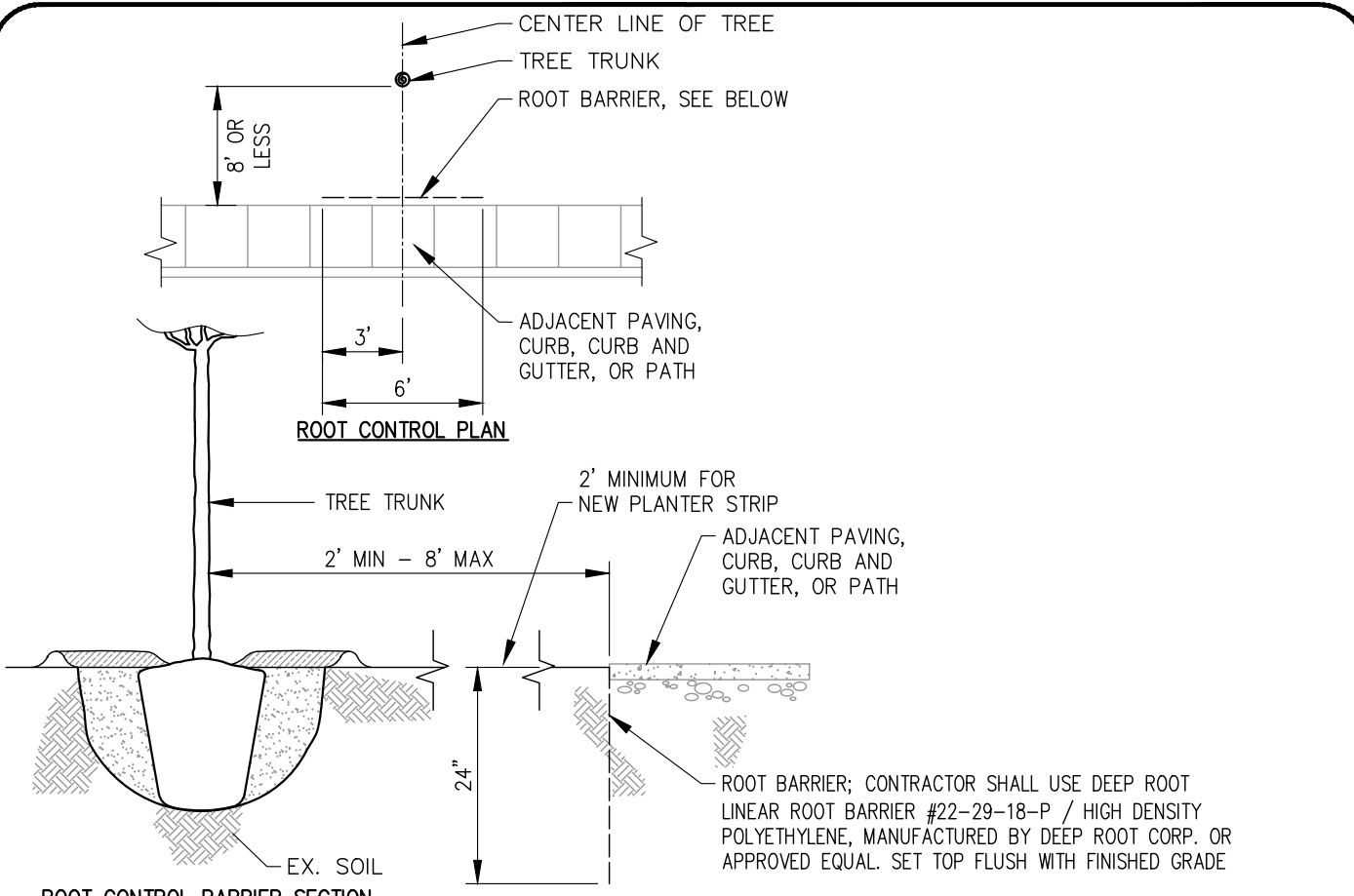
TREE PLANTING AND STAKING SECTION

NO.	REVISED	BY	APP.


	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DWN: CLG	STREET TREE PLANTING	
	CHK JT		
DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	ST 9-1 PAGE 1 OF 3 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 9-1.dwg
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Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 9-2.dwg Layout Name: ST 9-2 Plot Date: Dec 07, 2021 at 13:35



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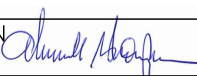
	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	STREET TREE PLANTING	ST 9-2
DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 2 OF 3 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 9-3.dwg
 Layout Name: ST 9-3 Plot Date: Dec 07, 2021 at 13:36

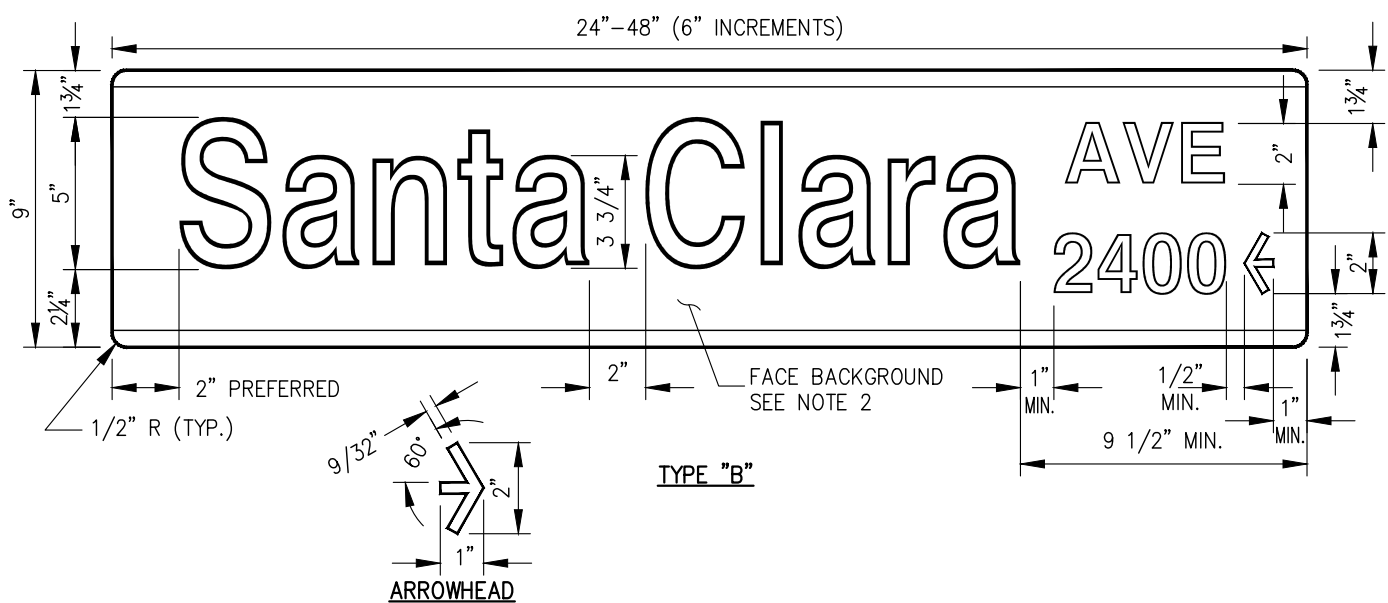
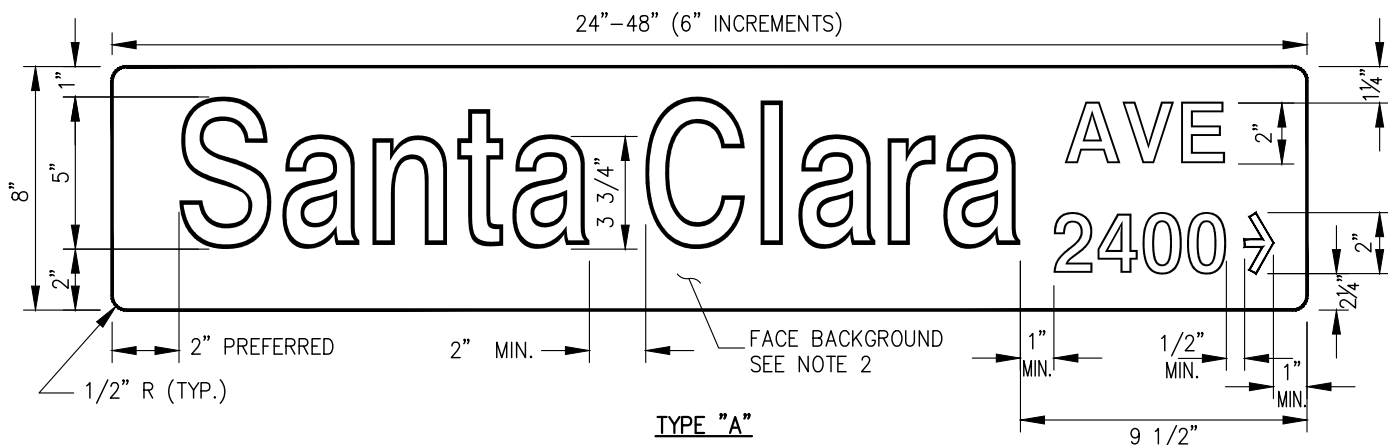
TREE PLANTING NOTES:

1. ALL WORK SHALL BE PERFORMED BY PERSONS FAMILIAR WITH THIS TYPE OF WORK AND UNDER THE SUPERVISION OF A QUALIFIED PLANTING FOREMAN.
2. CONTRACTOR SHALL VERIFY THAT ADEQUATE DRAINAGE EXISTS PRIOR TO PLANTING.
3. CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES PRIOR TO PLANTING.
4. TREES SHALL BE 7 TO 10 FEET HIGH SUPPLIED IN FIFTEEN GALLON CONTAINERS. TREES SHALL HAVE A MINIMUM OF 1.5 INCH TRUNK DIAMETER AT BREAST HEIGHT.
5. ALL TREES SHALL CONFORM TO THE STANDARDS SET FORTH IN THE MOST RECENT *AMERICAN STANDARDS FOR NURSERY STOCK* PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN (A.A.N.).
6. TREES SHALL HAVE A STRAIGHT STRONG TERMINAL LEADER, UN CUT AND UNBROKEN. THE MAIN TRUNK SHALL HAVE ACCEPTABLE LATERAL GROWTH ALONG ITS LENGTH SHORTER AND SMALLER IN DIAMETER THAN THE MAIN TRUNK.
7. ALL TREES THAT, IN THE OPINION OF THE CITY ENGINEER, ARE DISEASED, INSECT INFECTED, OR HAVE GIRDLING ROOTS, WILL BE REJECTED.
8. TREES WITH ROOT FLARE COVERED BY MORE THAN 1.5 INCHES OF SOIL WILL BE REJECTED PRIOR TO INSTALLATION.
9. PLANTING MIXTURE FOR THE BACKFILL SHALL HAVE NO SOIL AMENDMENTS.
10. IN PARKWAY STRIPS, SET PLANTER TOP 1.5 INCHES BELOW SIDEWALK GRADE. IN LAWNS OR GROUND COVER AREAS, SET PLANTER TOP FLUSH WITH FINISHED GRADE. IN LAWN AREAS THERE SHALL BE A 2 FOOT MULCH RING CLEARANCE BETWEEN EDGE OF LAWN AND TREE.
11. STAKING OF TREE IS NOT RECOMMENDED, EXCEPT ON WINDY SITES, FOR LARGE EVERGREEN TREES, OR IN AREAS WITH HEAVY TRAFFIC. IF STAKING IS DONE, FLEXIBLE HOSE, NOT TIES AND WIRES, SHALL BE USED. NAIL FLEXIBLE TREE STRAP TO EACH SIDE OF THE STAKE IN TEARDROP FIGURE WITH 1 INCH MAX GALVANIZED ROOFING NAIL.
12. TREE STAKES SHALL BE REMOVED AFTER TWO YEARS.
13. AT PLANTING, PRUNE ONLY CROSSING LIMBS, CO-DOMINANT LEADER, BROKEN, DISEASED OR DEAD BRANCHES, AND ANY BRANCHES THAT POSE A HAZARD TO PEDESTRIANS WHILE PRESERVING FORM AND CHARACTER OF TREE. DO NOT CUT LEADER. DO NOT PRUNE IN ORDER TO REDUCE CANOPY SIZE.
14. DO NOT WRAP TRUNK OF TREE.
15. A ROOT COLLAR EXCAVATION FOR ALL TREES SPECIFIED WILL BE DONE BY THE CITY ENGINEER TO ENSURE THAT TREES WERE NOT PLANTED OR GROWN TOO DEEPLY AT THE NURSERY. CONTRACTOR SHALL HAVE SUPPLIERS MARK GROUND LEVEL LINE ABOVE ROOT BALL. IF CITY ENGINEER DETERMINES THAT THERE IS EXCESSIVE SOIL OVER THE ROOT CROWN, THE TREES WILL BE REJECTED.
16. MULCH SHALL BE 3" DEEP UNLESS OTHERWISE NOTED.
17. TREES SHALL BE WATERED TWICE WEEKLY. ALL TREES NOT MEETING WITH THE APPROVAL OF THE CITY ENGINEER AT THE END OF THAT PERIOD SHALL BE REPLACED BY THE CONTRACTOR.
18. INSTALL TWO DEEP WATERING PERFORATED PLASTIC PIPES AS SHOWN. FILL PIPES WITH 3/4 INCH CLEAN DRAIN ROCK.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	STREET TREE PLANTING	ST 9-3
	DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 10-1.dwg Layout Name: ST 10-1 Plot Date: Dec 07, 2021 at 13:37



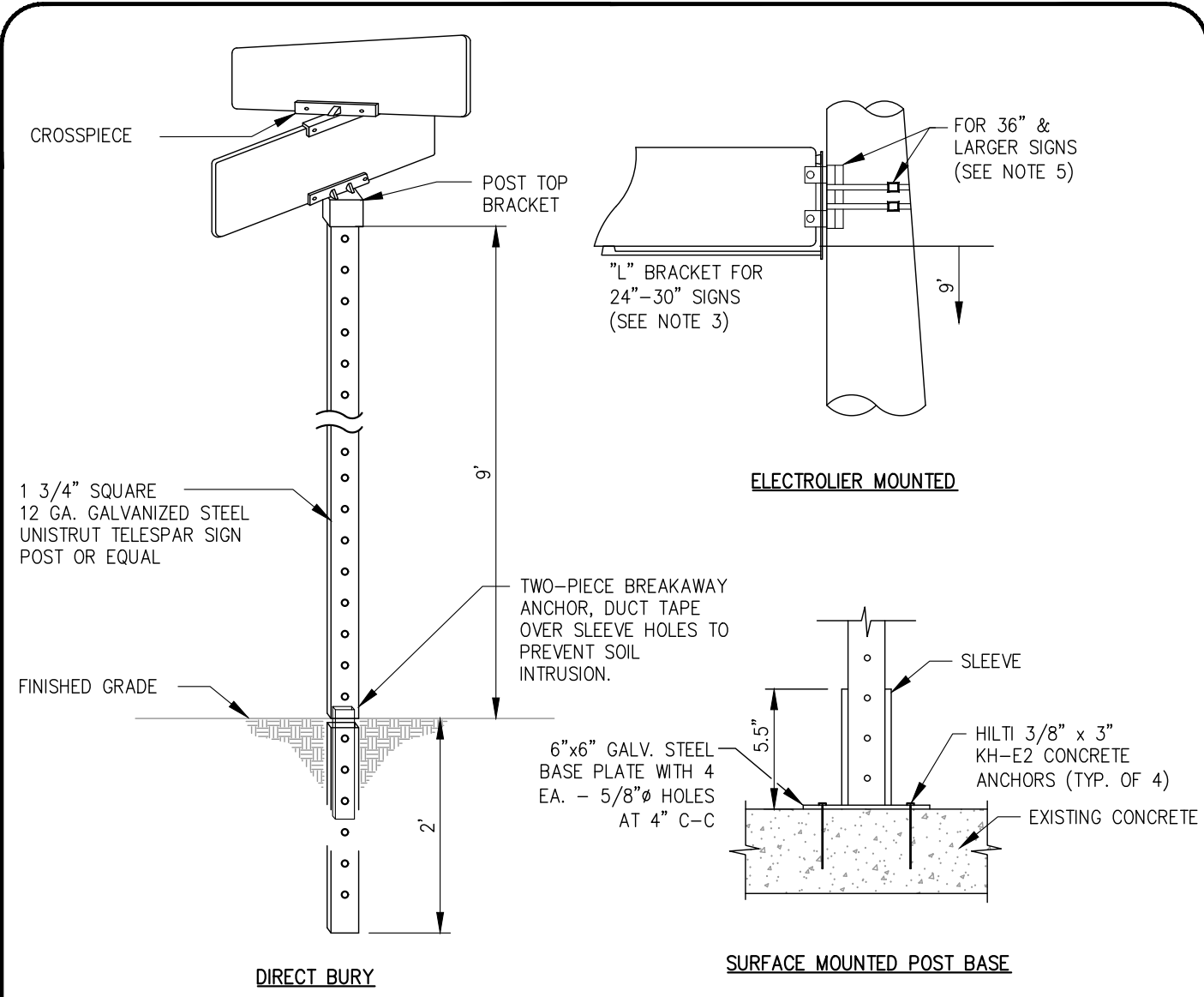
NOTES:

- STREET NAME BLADES SHALL BE 0.080" GAUGE, 6061-T6 ALUMINUM ALLOY (ASTM SPECIFICATION B221).
- FACE BACKGROUND SHALL BE INTERSTATE GREEN HIGH INTENSITY GRADE PRISMATIC REFLECTIVE SHEETING MATERIAL, CONFORMING TO THE CALTRANS SPECIFICATIONS.
- LEGEND SHALL BE SILVER SUPER ENGINEER GRADE (SEG) REFLECTORIZED SHEETING MATERIAL. THE STREET NAME SHALL BE COMPOSED OF 5" UPPER CASE AND 3 3/4" LOWER CASE LETTERS, FHWA SERIES B OR C. STREET NAME SUFFIXES SHALL BE 2" UPPER CASE LETTERS, FHWA SREIES C. BLOCK NUMBERS SHALL BE 2", FHWA SERIES C.
- ALL LETTER LAYOUTS SHALL CONFORM TO THE APPROPRIATE SPACING CHARTS OF THE FEDERAL HIGHWAY ADMINISTRATION. THE STREET NAME SUFFIX SHALL BE CENTERED BETWEEN THE STREET NAME AND THE EDGE OF THE SIGN. SIGNS SHALL BE IN 6" INCREMENTS OF LENGTH BETWEEN 24" AND 48", THE MINIMAL LENGTH BEING USED. THE STREET NAME SHALL BE COMPOSED OF FHWA SERIES B LETTERS ONLY WHEN THEIR USE RESULTS IN A SHORTER BLADE LENGTH. ALL SIGNS SHALL BE UNIFORM AND PROPORTIONAL IN APPEARANCE.
 - IN CASE WHERE NORMAL LAYOUT RESULTS IN A SIGN LONGER THAN 48", THE LEGEND SHALL BE CONDENSED IN A UNIFORM AND PROPORTIONAL MANNER SUBJECT TO APPROVAL BY THE ENGINEER.
- FACE SHALL BE ADHERED TO BLADES. ALL SIGNS SHALL BE DOUBLE-FACED. SIGN FACES SHALL BE FREE FROM BLEMISHES, BLISTERS, CRACKS, ETC.
- THE WORD "BLIND" IN 2" UPPER CASE, FHWA SERIES C LETTERS SHALL BE SUBSTITUTED FOR THE BLOCK NUMBER AND ARROWHEAD WHEN SO SPECIFIED.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT STREET NAME SIGN	STD DETAIL
	CHK JT		ST 10-1
DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 1 OF 2
			SCALE: NONE


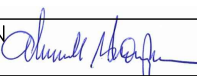
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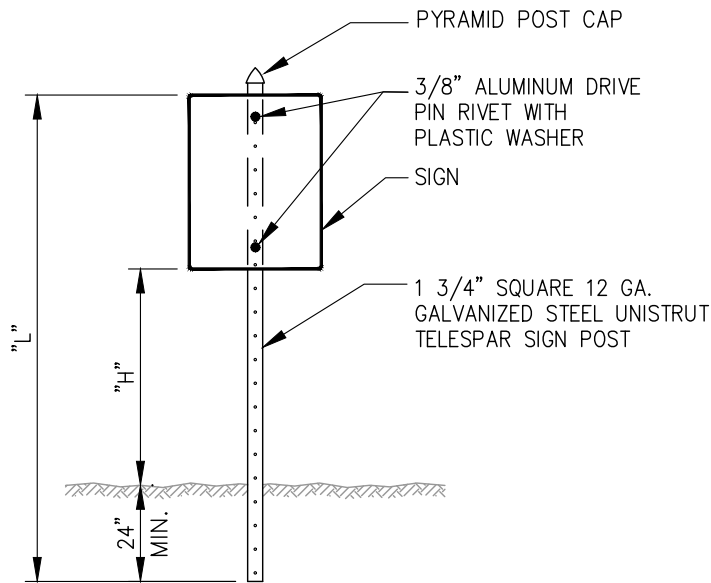


NOTES:

1. POST MOUNTING BRACKETS SHALL BE HAWKINS 109-00821 (5 1/4" FLAT SLOT) OR PACIFIC PRODUCTS & SERVICES #850 OR APPROVED EQUAL.
2. SIGN BLADE 90-DEGREE CROSS PIECES SHALL BE HAWKINS 037-0021 (5 1/2") PACIFIC PRODUCTS & SERVICES #850 OR APPROVED EQUAL.
3. ELECTROLIER MOUNTING "L" BRACKET SHALL BE HAWKINS V14F-SWB OR PACIFIC PRODUCTS & SERVICES #10-10 OR APPROVED EQUAL.
4. ALL MOUNTING BRACKETS SHALL BE OF HEAVY DUTY CAST ALUMINUM WITHOUT WELDED CONNECTIONS. BRACKETS SHALL BE COMPLETE WITH THEFT-PROOF ALLEN SET SCREWS.
5. SIGNS OVER 36" IN LENGTH SHALL BE MOUNTED WITH PACIFIC PRODUCTS & SERVICES #730 (9" LONG) BRACKET AND BE BOLTED TO THEIR MOUNTING BRACKETS USING STAINLESS STEEL 1/4" - 20 X 1 1/2" HEX CAP MACHINE SCREWS, STAINLESS STEEL 3/8" - 20 NUTS AND STAINLESS STEEL 3/8" LOCKWASHERS. A 1/4" DIAMETER HOLE IS TO BE DRILLED THRU THE BRACKET AND SIGN BLADE AT EACH SET SCREW LOCATION. BANDS FOR ELECTROLIER MOUNTING SHALL BE STAINLESS STEEL, 1/2" WIDE AND .025" THICK. THESE SHALL BE SECURED WITH STAINLESS STEEL CLAMPS.
6. LOCATION OF STREET NAME SIGNS SHALL BE AS DIRECTED BY THE ENGINEER.

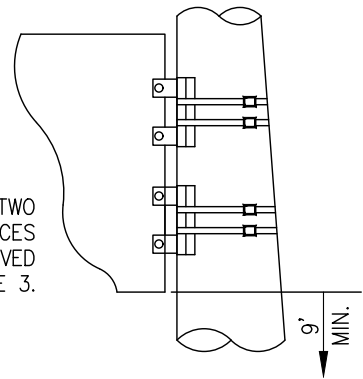
NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	STREET NAME SIGN		ST 10-2
DEC 2021	RUSS THOMPSON CITY ENGINEER	 DATE 12/07/21		PAGE 2 OF 2 SCALE: NONE



SEE CITY STD. ST 12 FOR "L" AND "H" DIMENSIONS.

TYPICAL SIGN INSTALLATION



ELECTROLIER MOUNTED USING TWO PACIFIC PRODUCTS & SERVICES #730 BRACKET OR APPROVED EQUAL. SEE NOTE 3.

ELECTROLIER MOUNTED SIGN DETAIL

NOTES:

- SIGN LEGEND AND SHAPE SHALL CONFORM WITH THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR EACH DESIGNATION SHOWN. SIGN SIZE, COLORS AND MATERIALS SHALL BE AS SPECIFIED IN CITY STD. ST 12.
- PARENTHESES () IN THE SIGN DESIGNATION INDICATE AN ARROW IN THE SIGN LEGEND. DIRECTION OF THE ARROW SHALL BE AS SHOWN ON THE CONSTRUCTION PLANS.
- ALL SIGNS, EXCEPT THE R26, R26(S), R28 (), AND THE R32 SIGNS SHALL BE SINGLE-FACED AND CENTER-POST MOUNTED. THE R26, R26(S), AND R32 SIGNS SHALL BE DOUBLE-FACED, THE R28 () SIGN SHALL BE SINGLE-FACED, AND THESE SHALL BE MOUNTED ON STANDARD "L-BRACKET" MOUNTINGS.
- ALL SIGNS SHALL BE ALUMINUM ALLOY WITH REFLECTIVE SHEETING EXCEPT AS FOLLOWS:
 - THE TYPE N-4, TYPE N-5, TYPE K-1, AND TYPE L-1 SHALL BE ALUMINUM ALLOY WITH NON-REFLECTIVE SHEETING.
 - THE TYPE N-4, TYPE N-5, TYPE L-1, AND TYPE K-1 SHALL HAVE REFLECTOR BUTTONS.
- ALL SIGN MATERIALS AND FABRICATION SHALL BE IN CONFORMANCE WITH THE LATEST CALTRANS SPECIFICATIONS AND SHALL MEET WITH THE APPROVAL OF THE CITY ENGINEER. REFLECTIVE SHEETING SHALL BE 3M HIGH-INTENSITY GRADE PRISMATIC OR APPROVED EQUAL. NON-REFLECTIVE SHEETING SHALL BE 3M ELECTRO CUT FILM OR APPROVED EQUAL.
- ALL SIGNS SHALL CONFORM TO GAUGE (PANEL THICKNESS) AS PER CALTRANS SPECIFICATIONS.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT TRAFFIC SIGN INSTALLATION	STD DETAIL
	CHK JT		ST 11
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 12-1.dwg
 Layout Name: ST 12-1 Plot Date: Dec 07, 2021 at 13:40

SIGN	DESIGNATION	SIZE	COLORS LEGEND ON BACKGROUND	H	L
STOP	R1-1	30" OCTAGON	WHITE ON RED	7'	11'
SPEED LIMIT	R2-1	24" X 30"	BLACK ON WHITE	7'	11-1/2'
KEEP RIGHT (SYMBOL)	R4-7	24" X 30"	BLACK ON WHITE	5'	10'
STOP AHEAD	W3-1	30" X 30"	RED & BLACK ON YELLOW BLACK ON WHITE	7'	12'
ONE WAY	R6-1 R10-1 (L OR R)	36" X 12"	ON BLACK	4'	7'
DO NOT ENTER	R5-1	30" X 30" OR 36" X 36"	RED ON WHITE	7'	12'
NO RIGHT TURN	R3-1	24" X 24"	RED & BLACK ON WHITE	7'	12'
NO LEFT TURN	R3-2	24" X 24"	RED & BLACK ON WHITE	7'	12'
RIGHT LANE MUST TURN RIGHT	R3-7	20" X 32"	BLACK ON WHITE	7'	12'
NO PARKING ANY TIME	R26	12" X 18"	RED ON WHITE	7'	10-1/2"
NO STOPPING ANY TIME	R26 (S)	12" X 18"	RED ON WHITE	7'	10-1/2"
NO PARKING ANY TIME	R28A	12" X 18" OR 18" X 24"	RED ON WHITE	7'	10-1/2"
2 HOUR PARKING	R32	12" X 18"	GREEN ON WHITE	7'	10-1/2"
NO LEFT TURN	R33	24" X 36"	RED & BLACK ON WHITE	7'	12'
NO U TURN	R3-4	24" X 24"	RED & BLACK ON WHITE	8.5'	12-1/2'
TRUCK ROUTE	R14-1	24" X 24"	BLACK ON WHITE	7'	11'
YIELD	R1-2	36" SIDES	RED ON WHITE	7'	11-1/2'
YIELD HERE TO PED	R1-5	30" X 30"	RED & BLACK ON WHITE	7'	12'
RIGHT TURN ONLY	R3-5 (R)	24" X 30" OR 30" X 36"	BLACK ON WHITE	7'	12'
LEFT TURN ONLY	R3-5 (L)	24" X 30" OR 30" X 36"	BLACK ON WHITE	7'	12'
NO PEDESTRIAN XING	R9-3a	36" X 18"	BLACK ON WHITE	7'	10-1/2"
BIKE ROUTE	D11-1	24" X 18"	WHITE ON GREEN	7'	10-1/2"
"BEGIN" WITH (BIKE ROUTE)	M4-14	12" X 5"	WHITE ON GREEN	8.5'	11'
"END" WITH (BIKE ROUTE)	M4-6	8" X 5"	WHITE ON GREEN	8.5'	11'
CURVE (90°)	W1-1 OR W1-1A (L OR R)	30" X 30"	BLACK ON YELLOW	7'	12-1/2'
CURVE	W1-2 (L OR R)	30" X 30"	BLACK ON YELLOW	7'	12-1/2'

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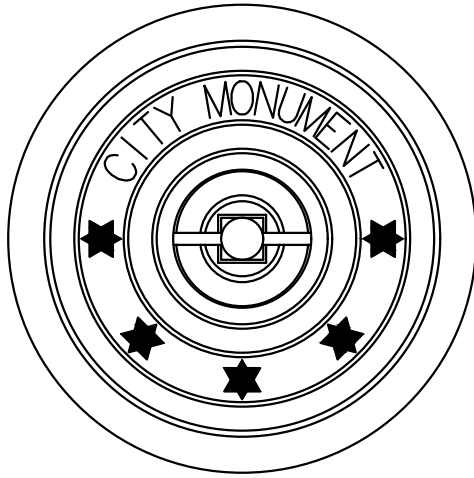
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	CHK JT		ST 12-1
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21

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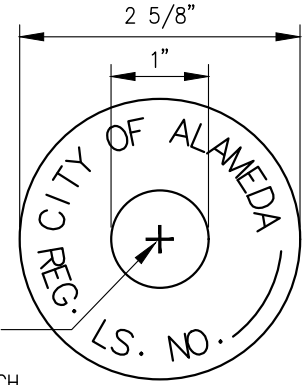
SIGN	DESIGNATION	SIZE	COLORS LEGEND ON BACKGROUND	H	L
T-INTERSECTION	W2-2 OR W2-4	30" X 30"	BLACK ON YELLOW	7'	12-1/2'
LANE REDUCTION	W4-2	30" X 30"	BLACK ON YELLOW	7'	11-1/2'
END	W31	24" X 24"	BLACK ON YELLOW	5'	9'
SIGNAL AHEAD NOT A THROUGH STREET NO OUTLET	W3-3	36" X 36"	BLACK ON YELLOW	7'	13-1/2'
TWO-WAY DIRECTIONAL ARROW	W14-2	24" X 24"	BLACK ON YELLOW	7'	12'
DIRECTIONAL ARROW	W6-3	30" X 30"	BLACK ON YELLOW	5'	8-1/2'
DIRECTIONAL ARROW	W1-6 OR 1-7	36" X 18"	BLACK ON YELLOW	5'	8-1/2'
MERGING TRAFFIC	W4-1	30" X 30"	BLACK ON YELLOW	7'	13-1/2'
SCHOOL	S1-1	30" X 30"	BLACK ON YELLOW	7'	12'
SCHOOL XING	S1-1 WITH W16-7P	30" X 30" 24" X 12"	BLACK ON YELLOW	8.5' 7'	14'
REFLECTOR	OM2-1 (TYPE K-1) TYPE L-1	15" X 6" 8" X 24"	YELLOW REFLECTORS ON WHITE	1' 2'	3-1/2' 6'
REFLECTOR	TYPE N-1	18' X 18"	YELLOW REFLECTIVE SHEETING W/ BLACK BORDER	2.5'	6-1/2'
REFLECTOR	TYPE N-2	18' X 18"	RED REFLECTIVE SHEETING W/BLACK BORDER	2.5'	6-1/2'
REFLECTOR	TYPE N	18' X 18"	ORANGE REFLECTIVE SHEETING W/BLACK BORDER	2.5'	6-1/2'
REFLECTOR (L OR T)	OM1-1 (TYPE N-4)	18' X 18"	YELLOW REFLECTORS ON YELLOW	2.5'	6-1/2'
REFLECTOR (END OF CUL-DE-SAC)	OM4-1 (TYPE N-5)	18' X 18"	RED REFLECTORS ON RED	2.5'	6-1/2'
BIKE LANE	R81	24" X 18"	BLACK ON WHITE	7'	10-1/2'
"BEGIN" WITH BIKE LANE	R81A	12" X 5"	BLACK ON WHITE	8.5'	11'
"END" WITH BIKE LANE	R81B	8" X 5"	BLACK ON WHITE	8.5'	11'

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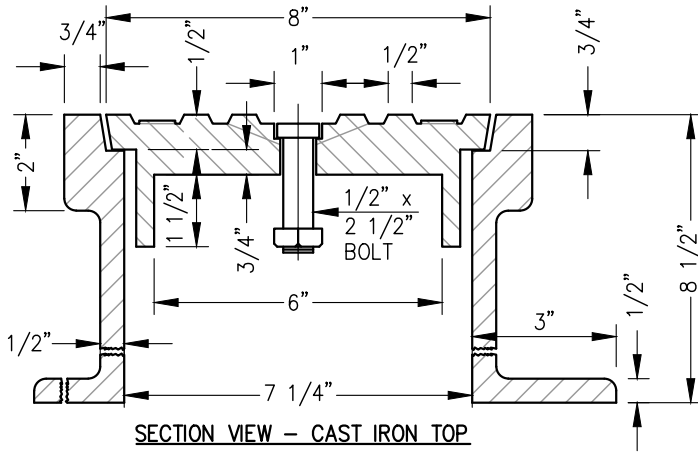
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	CHK JT		ST 12-2
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21



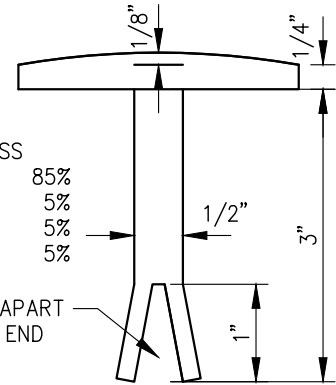
PLAN VIEW - CAST IRON TOP



REFERENCE MARK IN THE SHAPE OF A CROSS SHALL BE IN THE CENTER ONE INCH (1") DIA. AREA AND MARKED WITH A 3/8" STAR DRILL.



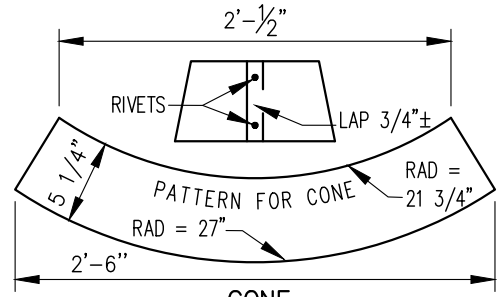
SECTION VIEW - CAST IRON TOP



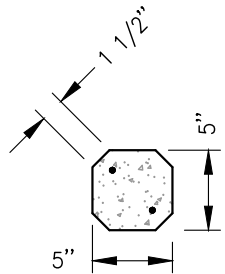
RED BRASS
COPPER 85%
TIN 5%
ZINC 5%
LEAD 5%

SPREAD APART
SLOTTED END

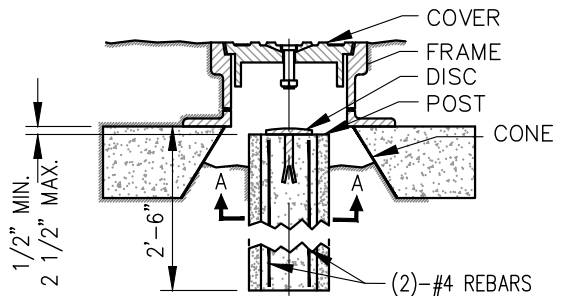
STANDARD DISC



CONE



SECTION A-A



STANDARD POST AND ASSEMBLY

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 13.dwg Layout Name: ST 13 Plot Date: Dec 07, 2021 at 13:47

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DWN: CLG
CHK JT
DEC 2021

CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT

SURVEY MONUMENT

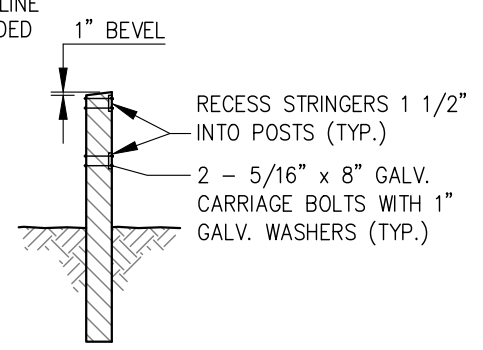
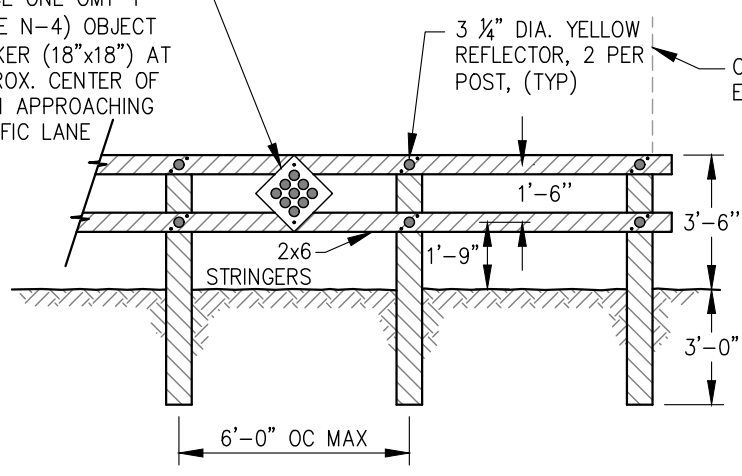
RUSS THOMPSON
CITY ENGINEER *Russ Thompson*

DATE 12/07/21

STD DETAIL
ST 13
PAGE 1 OF 1
SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 14.dwg Layout Name: ST 14 Plot Date: Dec 07, 2021 at 13:47


PLACE ONE OM1-1 (TYPE N-4) OBJECT MARKER (18"x18") AT APPROX. CENTER OF EACH APPROACHING TRAFFIC LANE



NOTES:

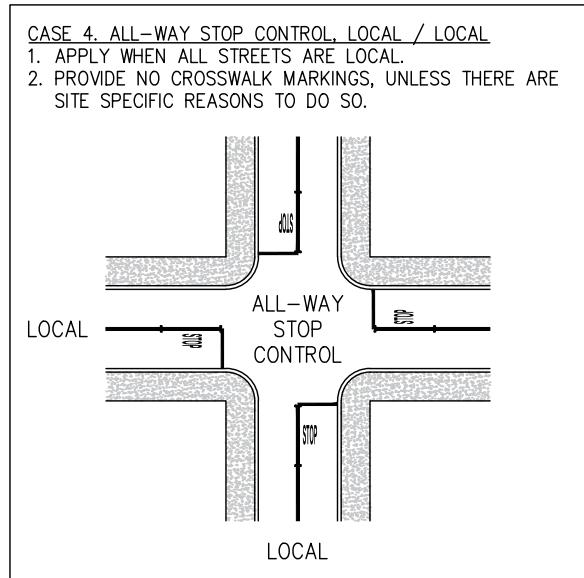
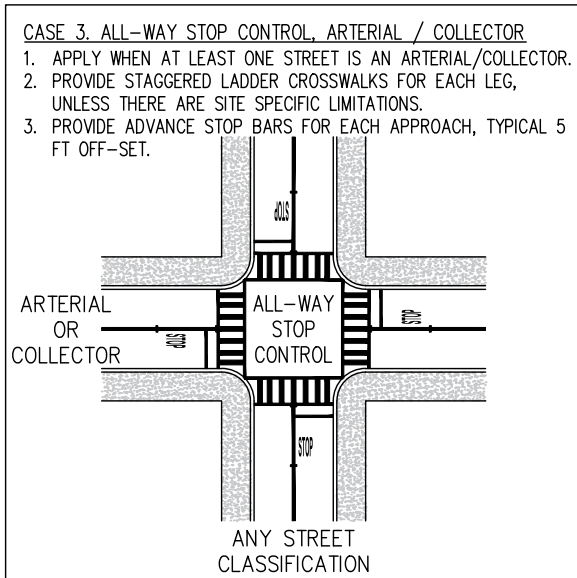
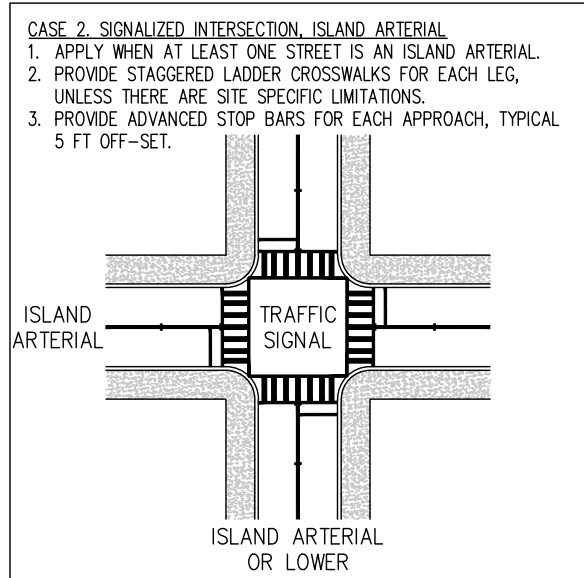
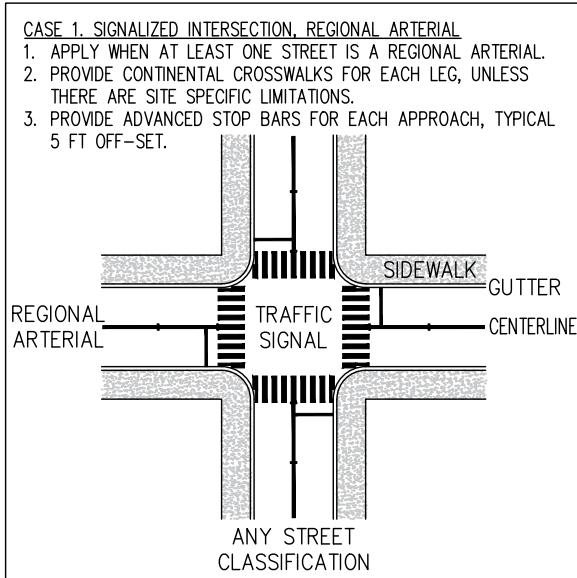
1. POSTS TO BE 6x6 STD OR BETTER PRESSURE-TREATED DOUGLAS FIR.
2. STRINGERS TO BE 2x6 STD OR BETTER PRESSURE-TREATED DOUGLAS FIR. SPLICE AT POST USING 2 BOLTS PER STRINGER.
3. BARRICADE TO BE PAINTED WITH TWO COATS EXTERIOR WHITE PAINT.

NO.	REVISED	BY	APP.


	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	DWN: CLG CHK JT	ST 14
	STREET BARRICADE RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	PAGE 1 OF 1
DEC 2021	DATE 12/07/21	SCALE: NONE

GENERAL NOTES

1. PEDESTRIAN RAMPS NOT SHOWN.
2. TYPICAL APPLICATIONS DO NOT DEPICT STANDARDS FOR CROSSWALK ALIGNMENT WITH RESPECT TO PEDESTRIAN RAMPS.
3. IT IS PREFERRED TO HAVE BI-DIRECTIONAL ACCESSIBLE PEDESTRIAN RAMPS FOR EACH CORNER OF AN INTERSECTION.
4. ACCESSIBLE PEDESTRIAN RAMPS SHALL BE PROVIDED AT ALL NEW CROSSWALKS.
5. STRIPING MAINTENANCE SHALL ENHANCE EXISTING CROSSWALKS, BUT NOT ADD NEW CROSSWALKS.
6. WHEN DETERMINING WHETHER A STREET OR APPROACH IS MULTI-LANE, TURN POCKETS AND TWO-WAY LEFT-TURN LANES SHALL BE COUNTED.

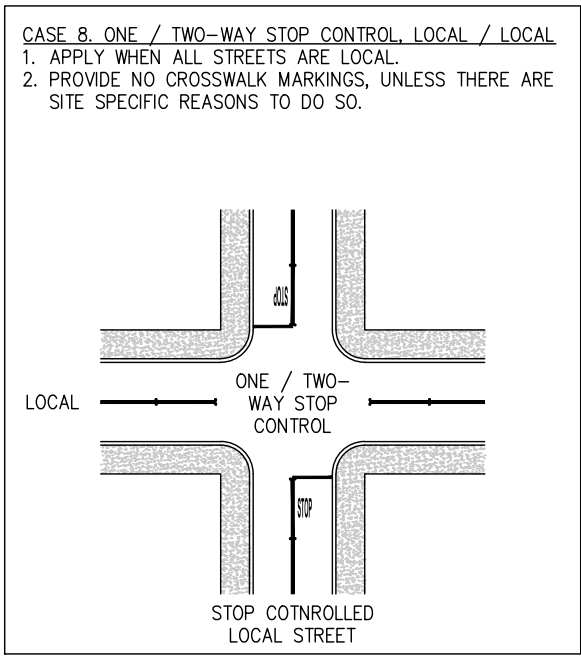
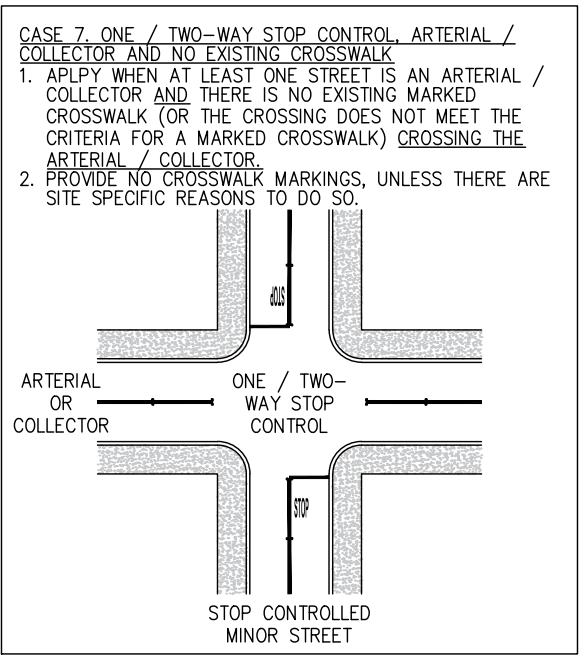
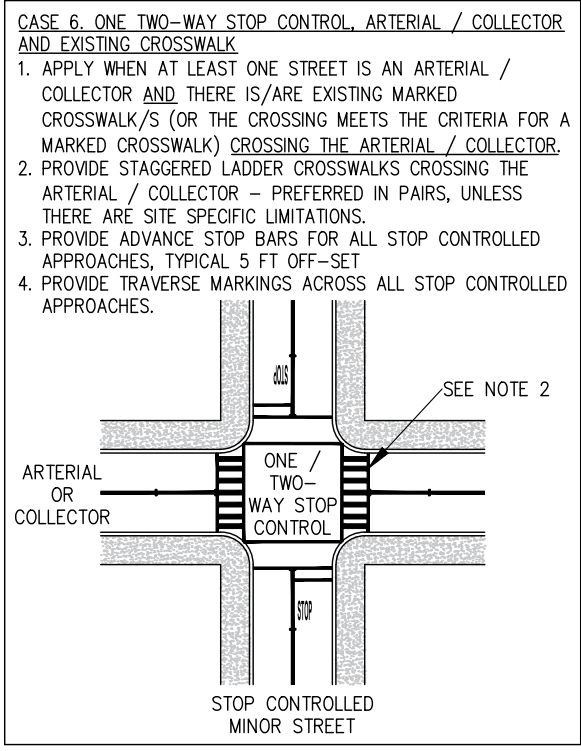
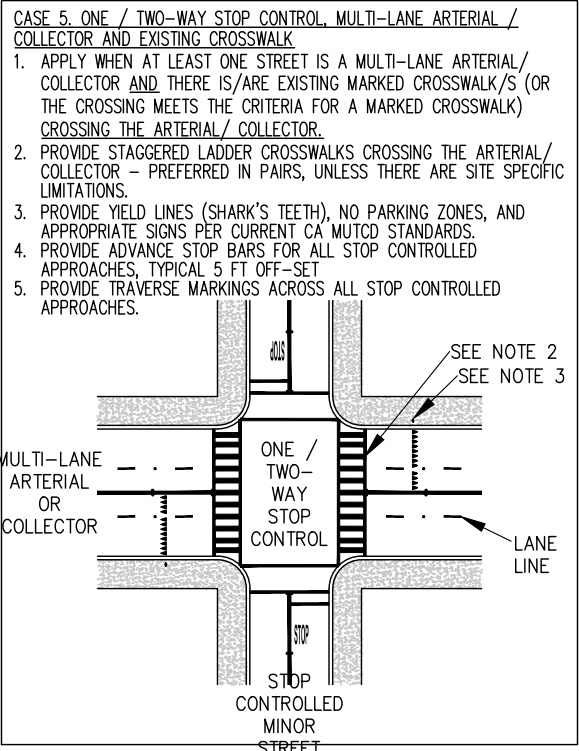


NO.	REVISED	BY	APP.


	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	TYPICAL APPLICATIONS FOR MARKED CROSSWALKS		ST 15-1
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 15-1.dwg Layout Name: ST 15-1 Plot Date: Dec 07, 2021 at 13:48

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 15-2.dwg
 Layout Name: ST 15-2 Plot Date: Dec 07, 2021 at 13:50

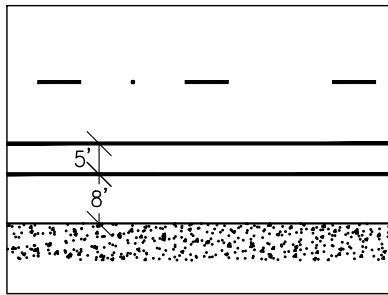


NO.	REVISED	BY	APP.

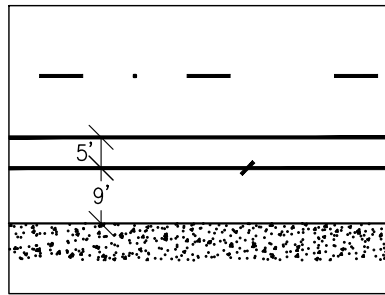
	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	TYPICAL APPLICATIONS FOR MARKED CROSSWALKS		ST 15-2
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 16.dwg Layout Name: ST 16 Plot Date: Dec 07, 2021 at 13:51

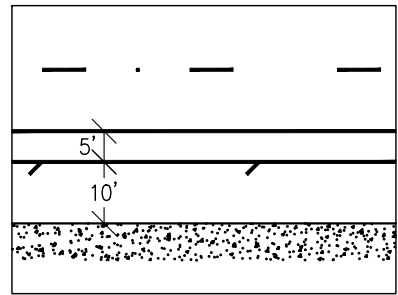
13' Width



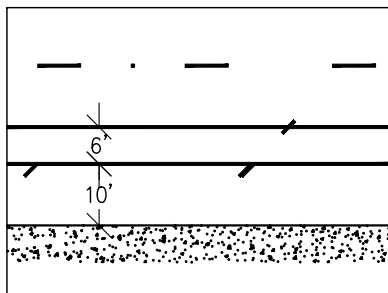
14' Width



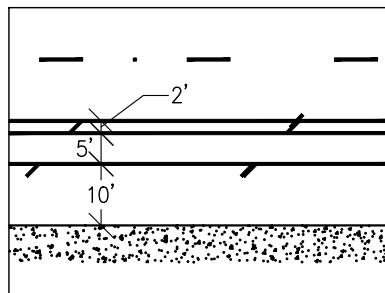
15' Width



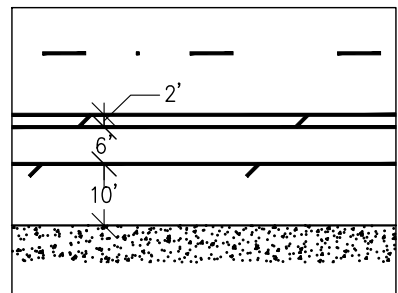
16' Width



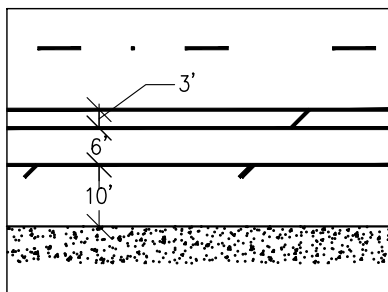
17' Width



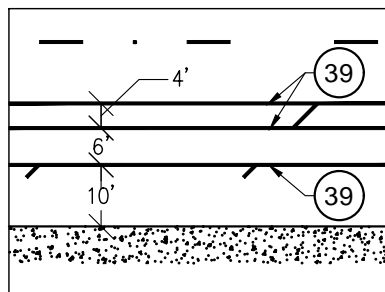
18' Width



19' Width




20' Width



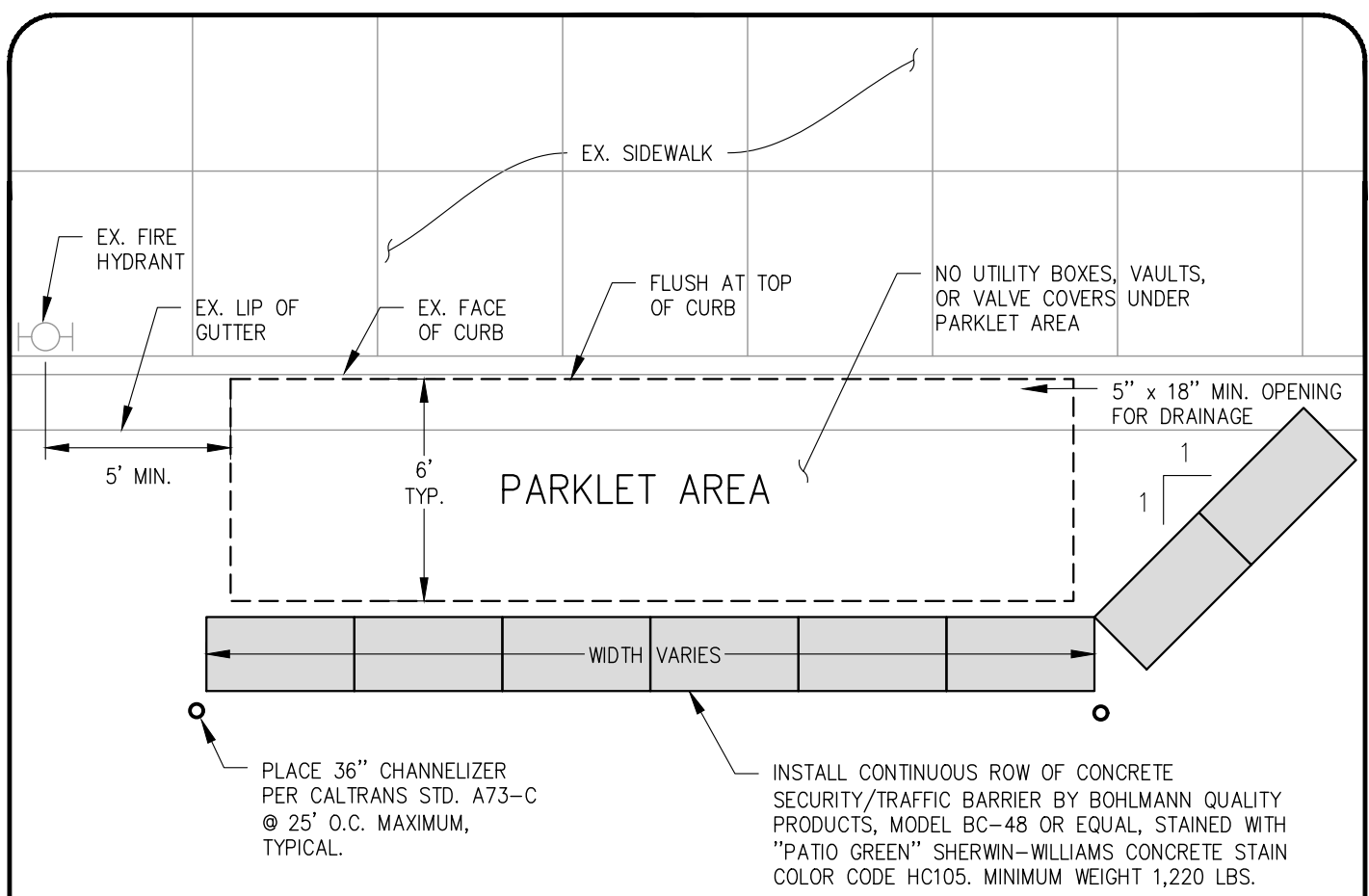
NOTES

1. **39** INDICATES DETAIL 39, WHICH IS A CONTINUOUS 6" WIDE, WHITE STRIPE.
2. BUFFER HATCHING SHALL BE 6" WIDE, WHITE STRIPES; AT 45°; AND SPACED EVERY 25' CENTER TO CENTER.
3. THE PARKING SIDE BUFFER, WHERE PRESENT, IS 2' WIDE, MEASURED PERPENDICULAR FROM THE CURB LINE.

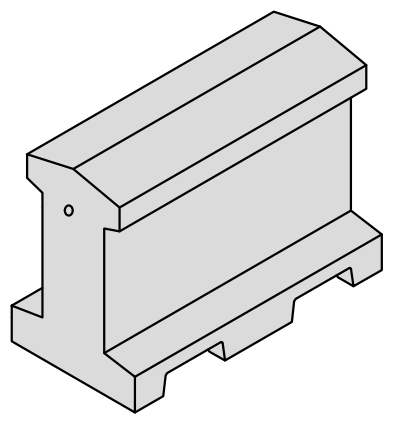
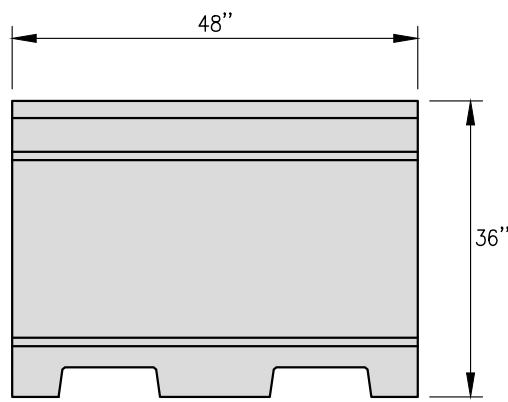
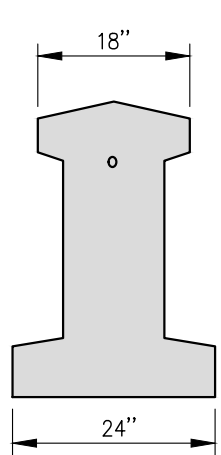
NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	BIKE LANE AND BUFFER WIDTHS (NON-METERED PARKING)		ST 16
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 1 OF 1 SCALE: NONE

Images: Alameda.png; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 17-1.dwg Layout Name: ST 17-1 Plot Date: Dec 07, 2021 at 13:52



PLAN VIEW



PRECAST CONCRETE BARRICADE
(BOHLMANN BC-48 OR EQUAL)

* NOTE: SEE "PARKLET DESIGN GUIDELINES" FOR OTHER REQUIREMENTS.

NO.	REVISED	BY	APP.

DWN: CLG
CHK JT
DEC 2021

CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT

PARKLET BARRICADE DETAIL

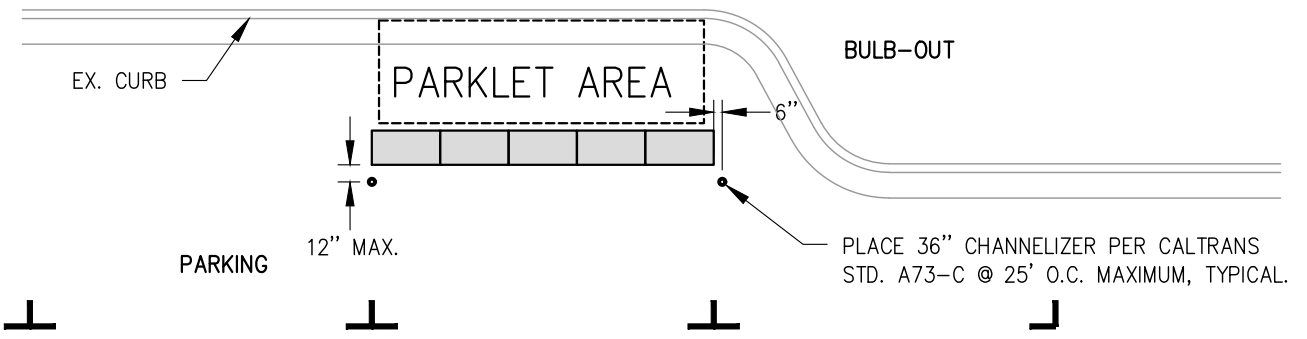
DATE 12/07/21

STD DETAIL

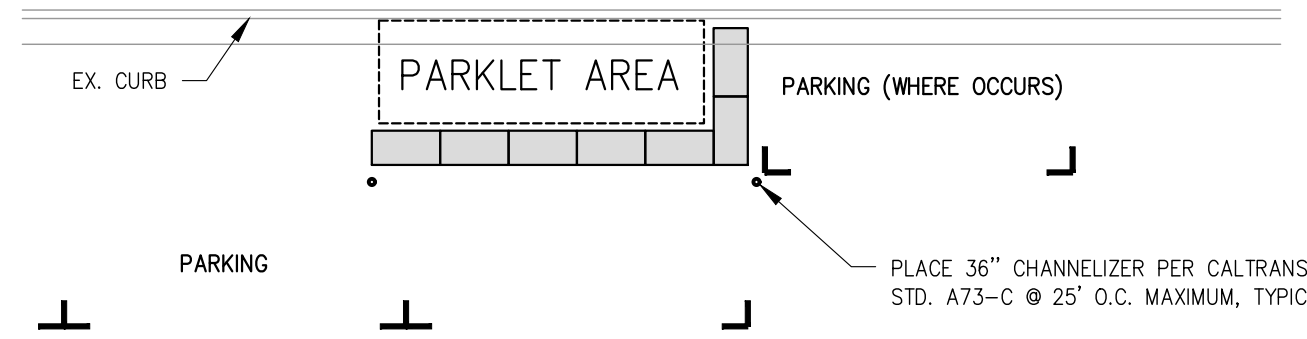
ST 17-1

PAGE 1 OF 1
SCALE: NONE

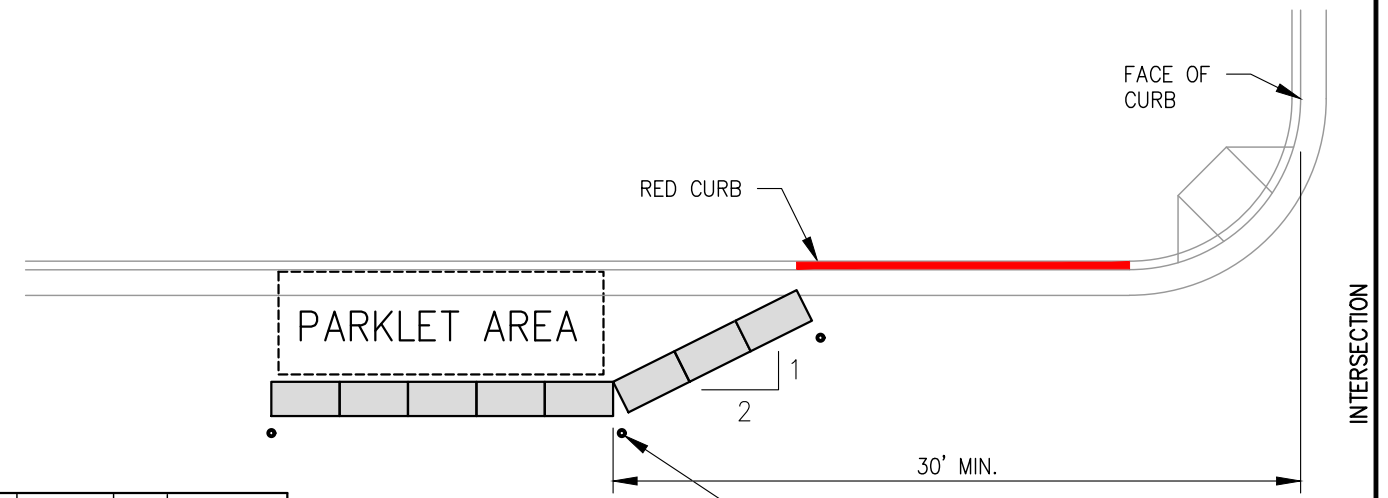
Images: Alameda.png; Xrefs:
 Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 17-2.dwg Layout Name: ST 17-2 Plot Date: Dec 07, 2021 at 13:53



CASE 1




CASE 2



CASE 3

NO.	REVISED	BY	APP.

	DWN: CLG
	CHK JT
	DEC 2021

CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT

PARKLET BARRICADE DETAIL

DATE 12/07/21

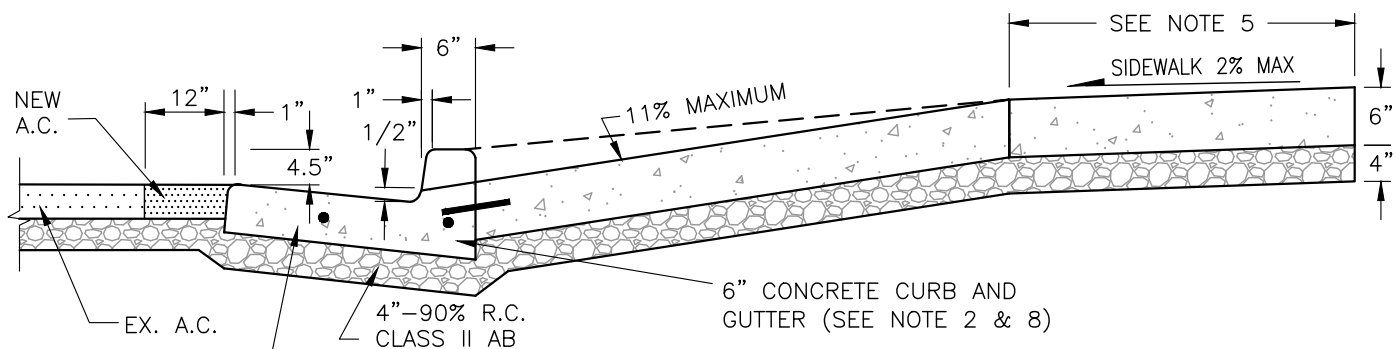
STD DETAIL

ST 17-2

PAGE 1 OF 1

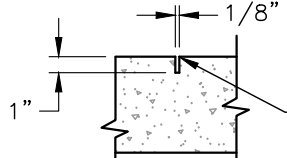
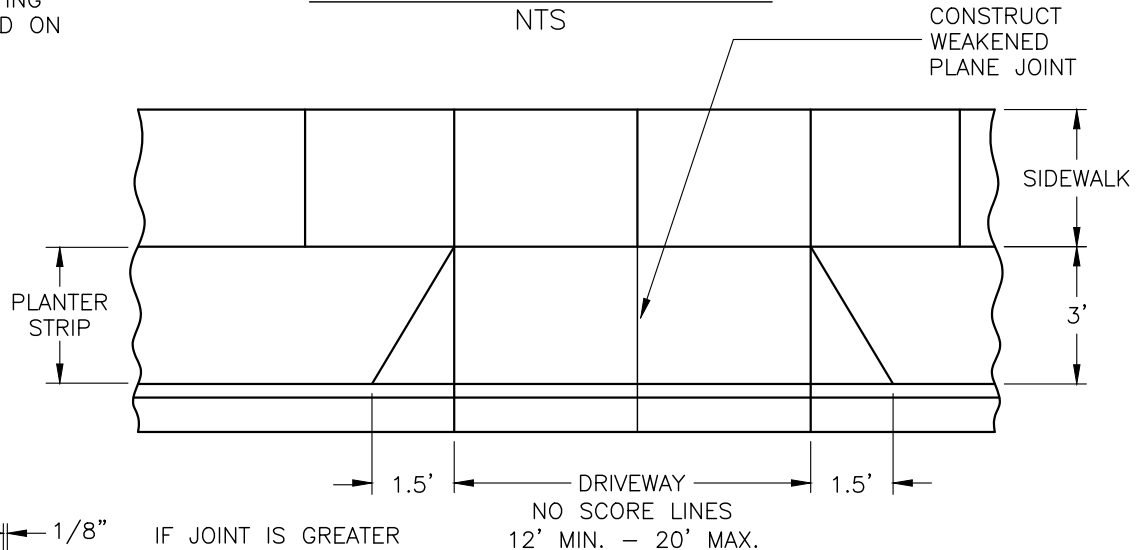
SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 18.dwg Layout Name: ST 18 Plot Date: Dec 07, 2021 at 13:53



CURB AND GUTTER TO MATCH EXISTING OR AS SPECIFIED ON THE PLANS.

RESIDENTIAL DRIVEWAY SECTION
NTS




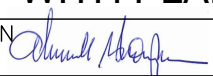
IF JOINT IS GREATER THAN 1/4" WIDE, IT SHALL BE FILLED WITH POLYURETHANE CONCRETE JOINT SEALANT.

DRIVEWAY PLAN
NTS

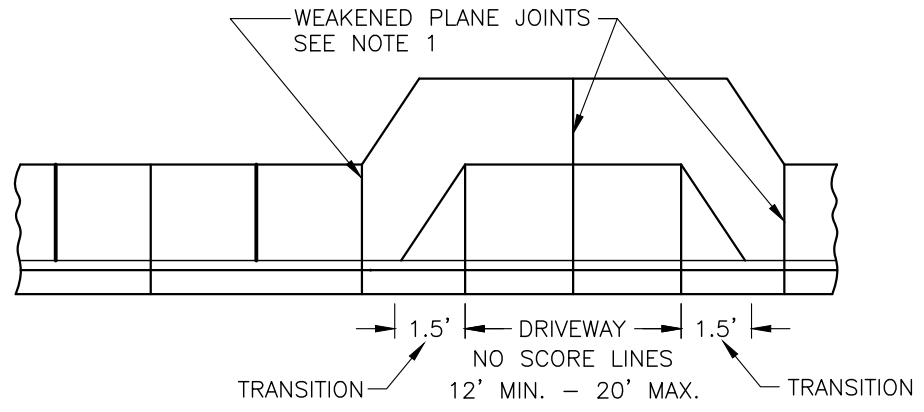
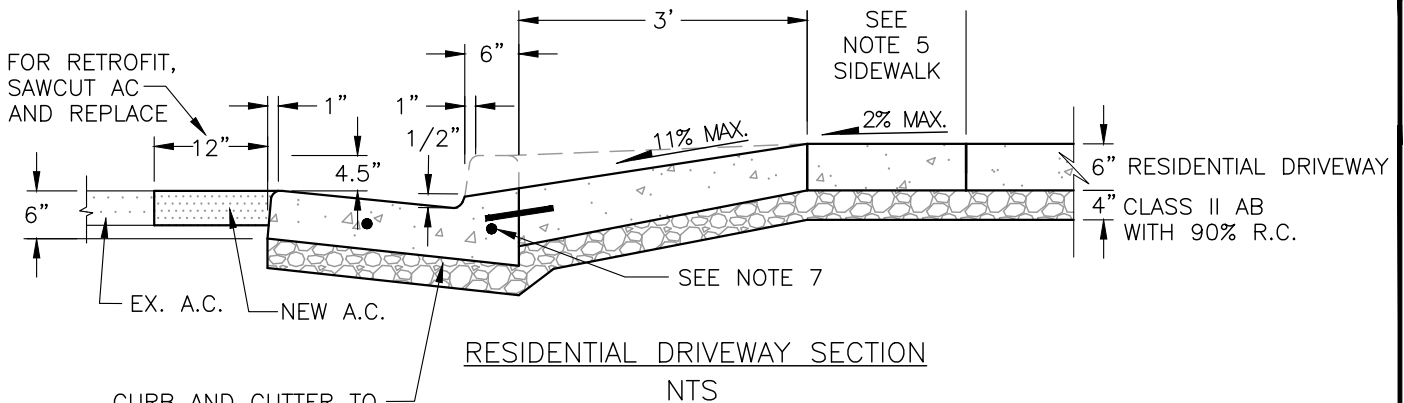
NOTES:

1. CONSTRUCT WEAKENED PLANE JOINTS AT 20' INTERVAL TO MATCH SCORE LINES.
2. SEE STD. DETAIL ST 1-2 FOR CONCRETE REQUIREMENTS. MEDIUM BROOM TEXTURE FINISH.
3. CONSTRUCT EXPANSION JOINTS AT 100' MAX.
4. TOTAL WIDTH OF DRIVEWAY OPENINGS NOT TO EXCEED 40% OF LOT FRONTAGE.
5. MINIMUM WIDTH OF CLEAR PASSAGE SHALL BE 4 FT. WHERE RIGHT OF WAY RESTRICTIONS, NATURAL BARRIERS OR OTHER RESTRICTIONS CREATE AN UNREASONABLE HARDSHIP, THE CLEAR WIDTH MAY BE REDUCED TO 3FT., WITH APPROVAL OF THE PUBLIC WORKS DEPARTMENT.
6. ALL EXPOSED EDGES SHALL BE ROUNDED TO 1/2" RADIUS.
7. NEW OR MODIFIED UTILITY POLES OR UTILITY EQUIPMENT MAY BE LOCATED NO CLOSER THAN 10 FEET OF A DRIVEWAY TO AVOID SIGHT-LINE OBSTRUCTIONS.
8. 12" #4 REBAR DOWELS, 3" MINIMUM DEPTH INTO EXISTING CURB & GUTTER. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	RESIDENTIAL DRIVEWAY WITH PLANTING STRIP	ST 18
	DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21

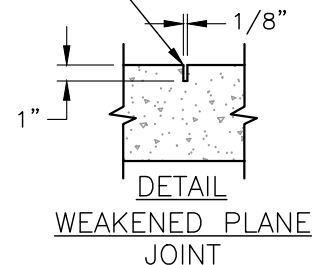
Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 19.dwg Layout Name: ST 19 Plot Date: Dec 07, 2021 at 13:54




NOTES:

1. CONSTRUCT WEAKENED PLANE JOINTS AT 20' INTERVAL TO MATCH SCORE LINES.
2. SEE STD. DETAIL ST 1-2 FOR CONCRETE REQUIREMENTS. MEDIUM BROOM TEXTURE FINISH.
3. CONSTRUCT EXPANSION JOINTS AT 100' MAX.
4. TOTAL WIDTH OF DRIVEWAY OPENINGS NOT TO EXCEED 40% OF LOT FRONTAGE.
5. MINIMUM WIDTH OF CLEAR PASSAGE SHALL 4 FT. WHERE RIGHT OF WAY RESTRICTIONS, NATURAL BARRIERS OR OTHER RESTRICTIONS CREATE AN UNREASONABLE HARDSHIP, THE CLEAR WIDTH MAY BE REDUCED TO 3FT., WITH APPROVAL OF THE PUBLIC WORKS DEPARTMENT.
6. ALL EXPOSED EDGES SHALL BE ROUNDED TO 1/2" RADIUS.
7. 12" #4 REBAR DOWELS, 3" MINIMUM DEPTH INTO EXISTING CURB & GUTTER. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.

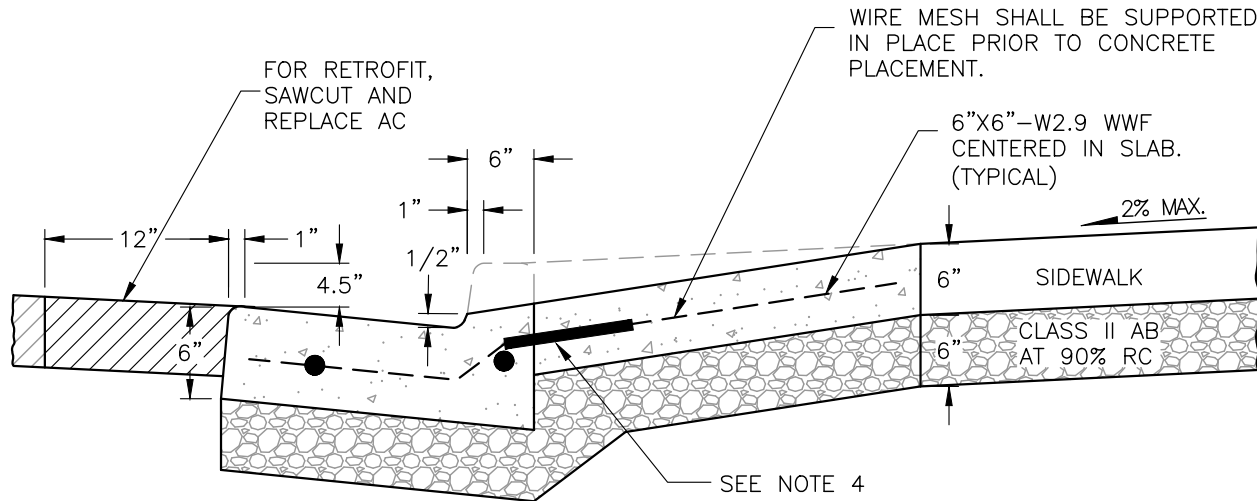
IF JOINT IS GREATER THAN 1/4" WIDE JT SHALL BE FILLED WITH POLYURETHANE CONCRETE JOINT SEALANT.



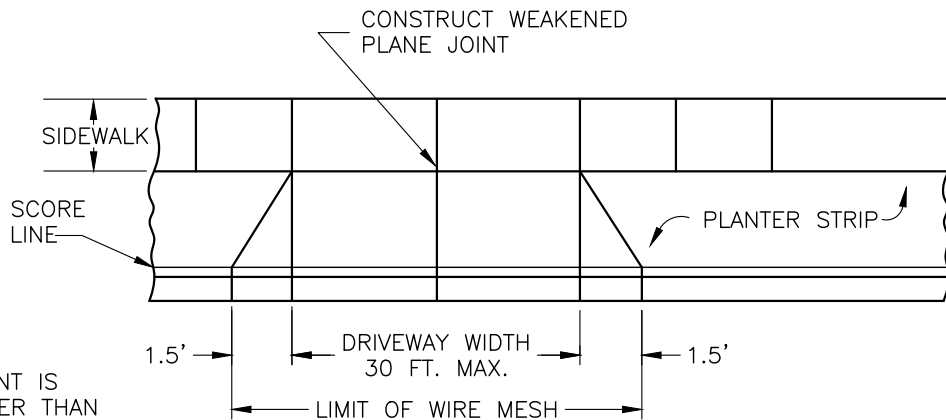
NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	RESIDENTIAL DRIVEWAY WITH MONOLITHIC SIDEWALK	ST 19
	DWN: CLG CHK JT DEC 2021 RUSS THOMPSON CITY ENGINEER	DATE 12/07/21 SCALE: NONE

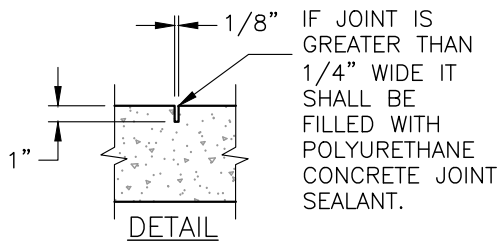
Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\ST 20.dwg Layout Name: ST 20 Plot Date: Dec 07, 2021 at 13:56



SECTION
NTS



DRIVEWAY PLAN
NTS



DETAIL
WEAKENED PLANE JOINT

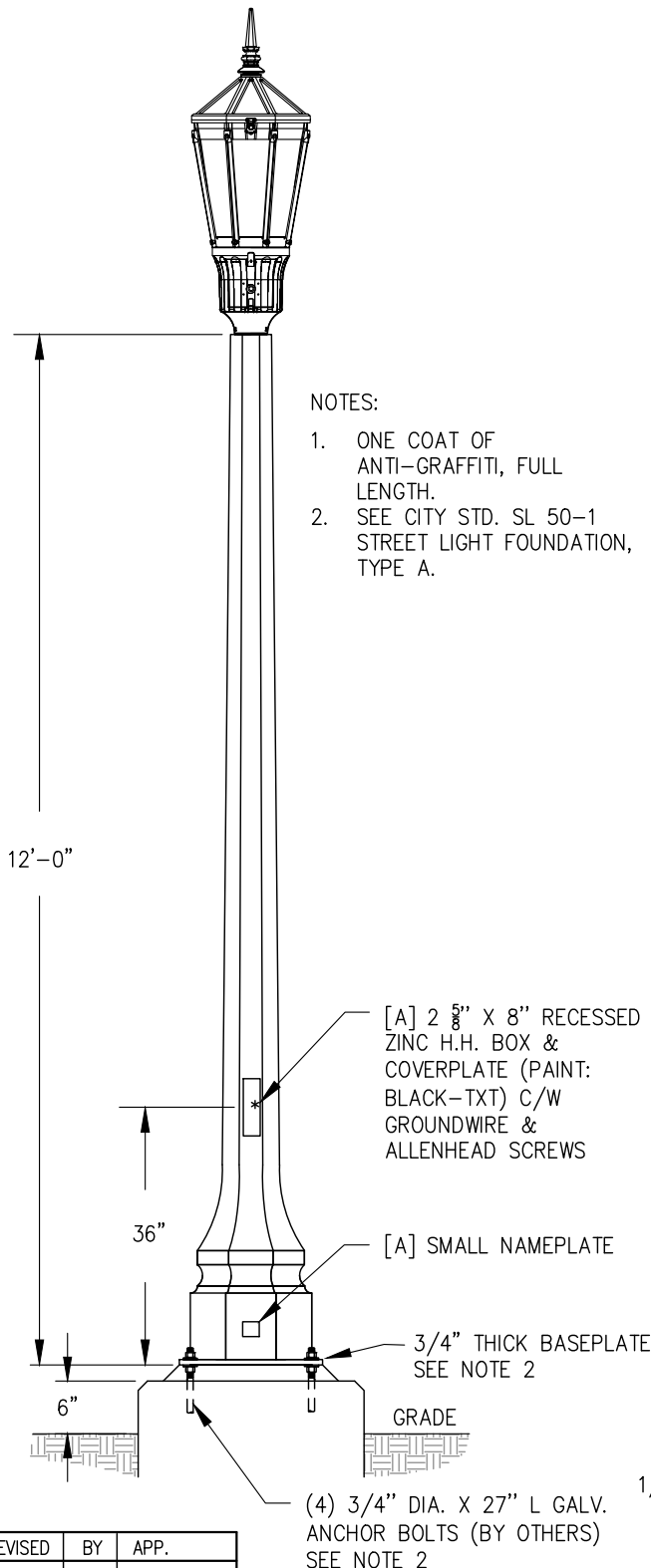
NOTES:

1. SEE STD. DETAIL 101 FOR CONCRETE REQUIREMENTS. LIGHT BROOM FINISH WITH IMPERVIOUS MEMBRANE CURE.
2. ALL EXPOSED EDGES SHALL BE ROUNDED TO 1/2" RADIUS.
3. #4 BARS SPACED AT 6" O.C. MAY BE SUBSTITUTED FOR THE WIRE MESH WHEN APPROVED BY THE CITY ENGINEER.
4. 12" #4 REBAR DOWELS, 3" MINIMUM DEPTH INTO EXISTING CURB & GUTTER. DO NOT DOWEL PRIVATE IMPROVEMENTS INTO PUBLIC IMPROVEMENTS.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	COMMERCIAL TYPE GUTTER & DRIVEWAY		ST 20
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 1 OF 1 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 8.dwg Layout Name: SL 8 Plot Date: Dec 07, 2021 at 12:32



POLE SPECIFICATIONS

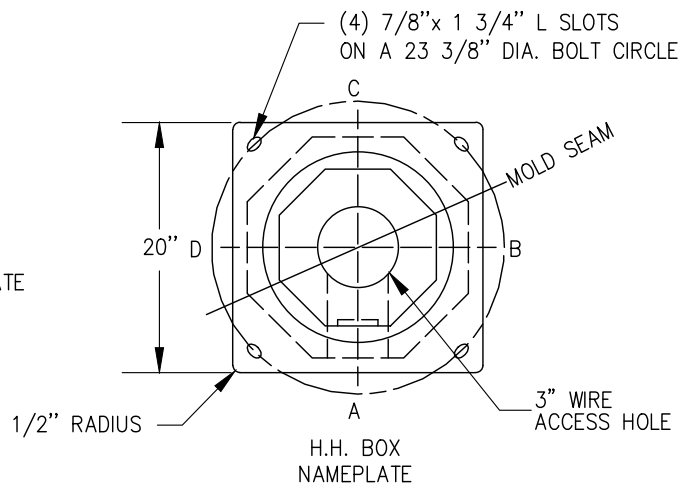
CATALOG NO.:	KBC12-G-S11-FBP-AG C/W 140-35/35
SECTION:	OCTAGONAL
COLOR:	ECLIPSE
FINISH:	POLISHED
POLE TOP:	5 1/2" FL/FL
POLE BUTT:	17" FL/FL
POLE LENGTH:	12' 0"
APPROX WEIGHT:	1,100 LBS.
MIN. RACEWAY:	1 1/4" ø
F'C @ 28 DAY PER ASTM C31:	8,000 PSI
WARRANTY:	LIFETIME

LUMINAIRE SPECIFICATIONS

CATALOG NO.:	K56-C-K24-P4AR-III/V-60W(SSL) -7030-120:277-PR7-#6-3K
LANTERN TYPE:	CLEVELAND (NO SPURS)
POLE ADAPTOR:	K24 CAPITAL
OPTICAL SYSTEM:	FLAT ARRAY ACRYLIC RIPPLED
IES LIGHTING CLASS:	TYPE III <input type="checkbox"/> TYPE V <input type="checkbox"/>
INPUT WATTS:	60W SOLID STATE LIGHTING
SERIES:	7030
CCT:	3000K
LINE VOLTAGE:	120:277V
PAINT:	TEXTURED BLACK
OPTIONS:	7-PRONG TWISTLOCK RECEPTACLE, (PHOTO-EYE BY OTHERS) ANSI C136.41 7 PIN RECEPTACLE

OPTIONS:

QUICK DISCONNECT	<input checked="" type="checkbox"/>
TERMINAL BLOCK	<input type="checkbox"/>



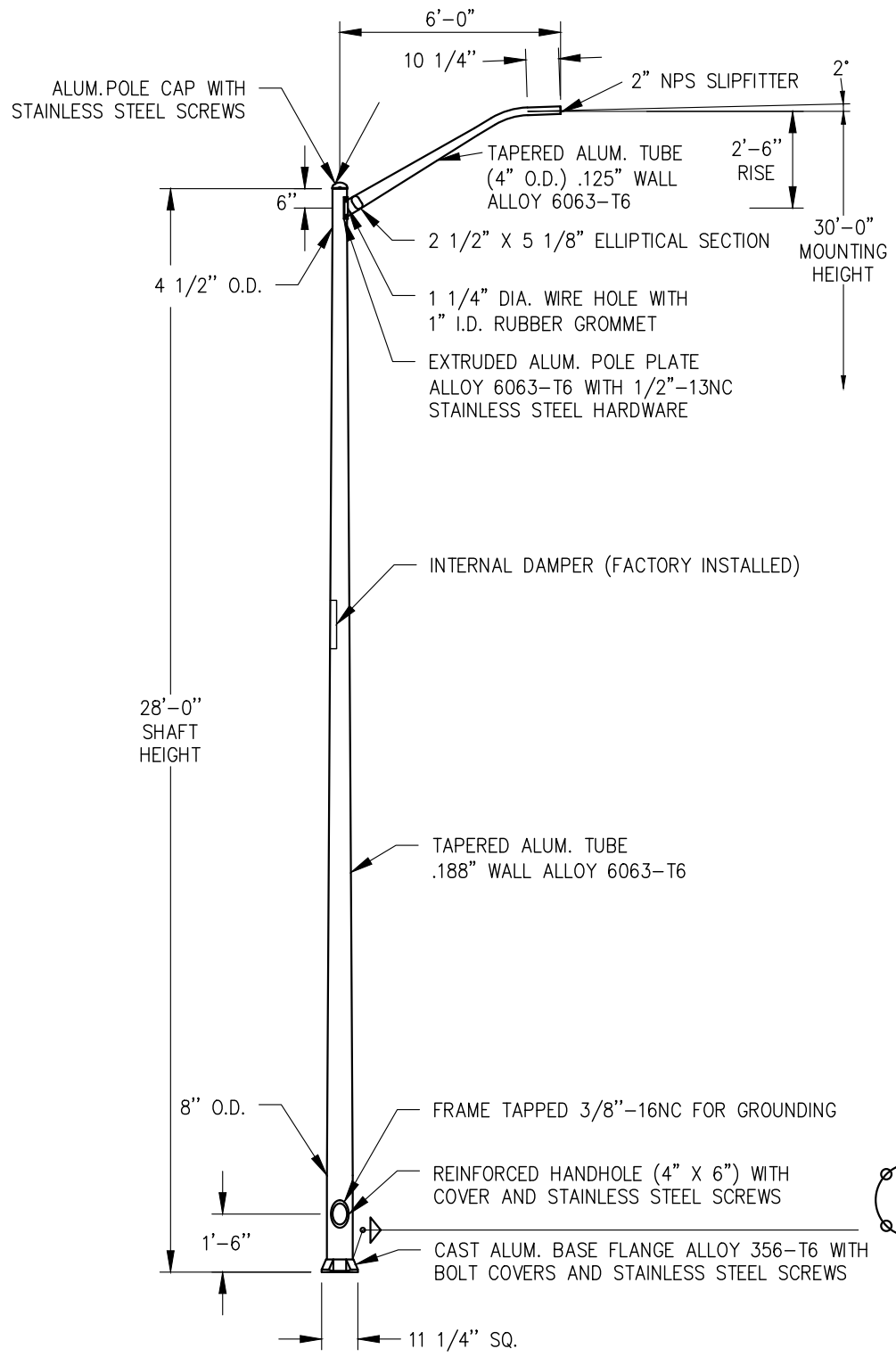
BASEPLATE DETAIL

20" x 20" x 3/4" THK. GALV BASEPLATE

NO.	REVISED	BY	APP.


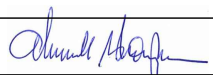
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	CHK JT	ALAMEDA RECREATION AND PARKS DEPT		SL 8
	DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>		PAGE 1 OF 1
			DATE 12/07/21	SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 9.dwg Layout Name: SL 9 Plot Date: Dec 07, 2021 at 12:34

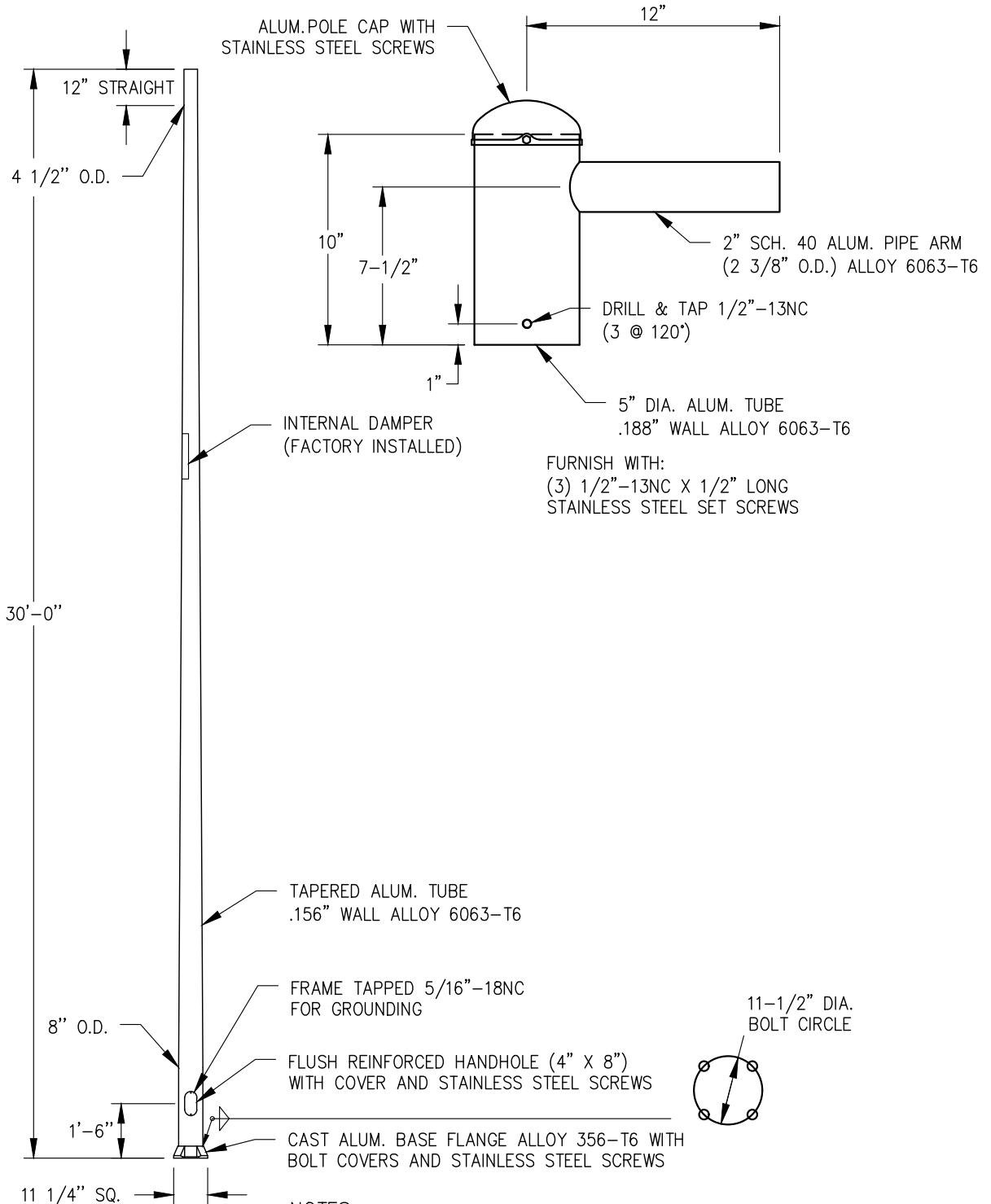


NOTES:
 SEE CITY STD. SL 50-1 STREET LIGHT FOUNDATION - TYPE A.
 POLE TO BE HAPCO B21-585 OR APPROVED EQUAL. DO NOT
 INSTALL LIGHTING POLES WITHOUT LUMINAIRES.

NO.	REVISED	BY	APP.


	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	28' TAPERED ALUMINUM POLE WITH 6' ARM		SL 9
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 10.dwg Layout Name: SL 10 Plot Date: Dec 07, 2021 at 12:38

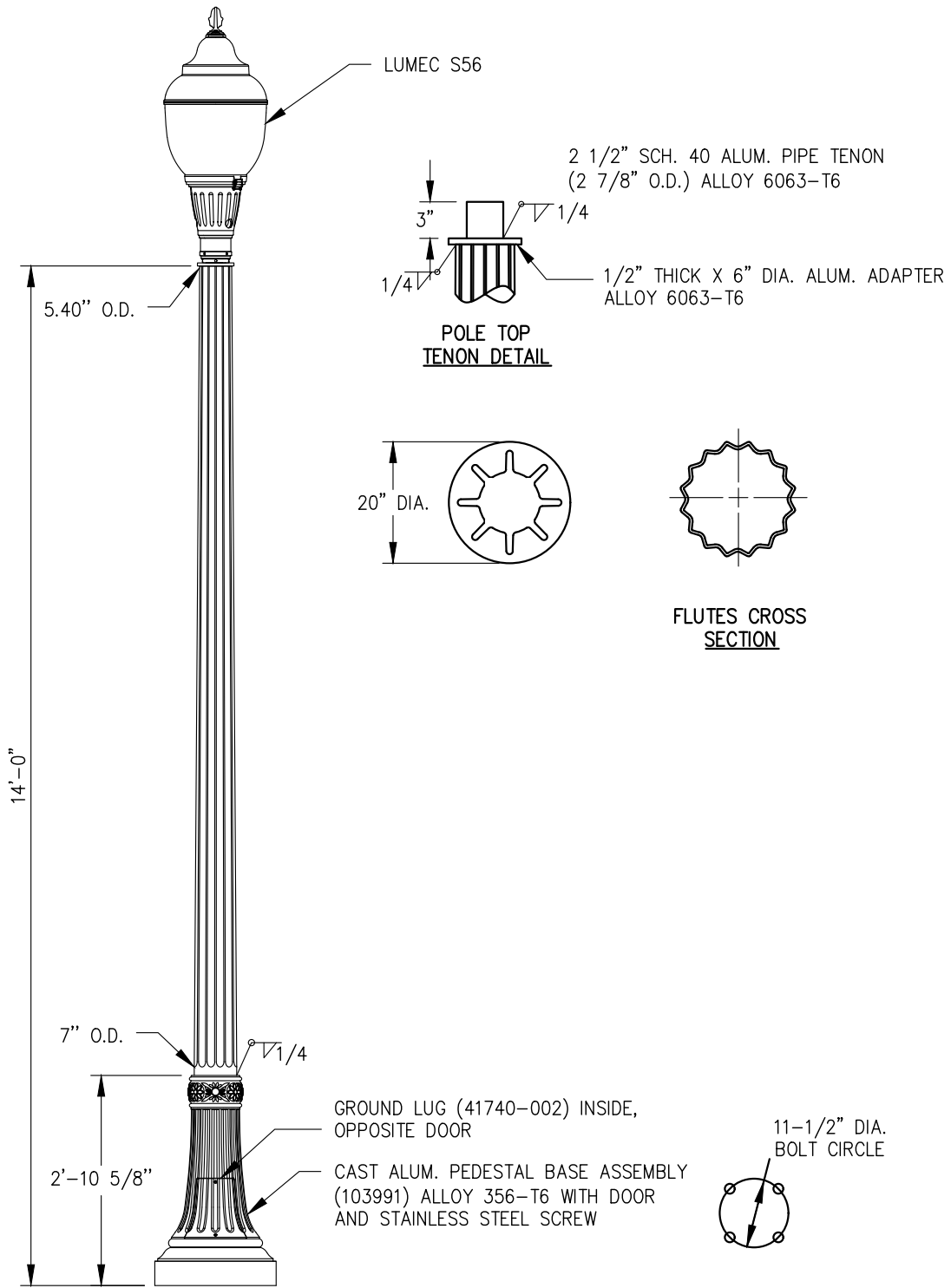


NOTES:
 SEE CITY STD. SL 50-1 FOR STREET LIGHT FOUNDATION - TYPE A.
 POLE TO BE HAPCO B103776 OR APPROVED EQUAL. HUB MOUNT TO
 BE HAPCO A103777 OR APPROVED EQUAL. DO NOT INSTALL LIGHTING
 POLES WITHOUT LUMINAIRES.

NO.	REVISED	BY	APP.


	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	30' TAPERED LIGHT POLE WITH 12" PIPE BRACKET ARM		SL 10
	DWN: CLG CHK: JT DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 11.dwg Layout Name: SL 11 Plot Date: Dec 07, 2021 at 12:39

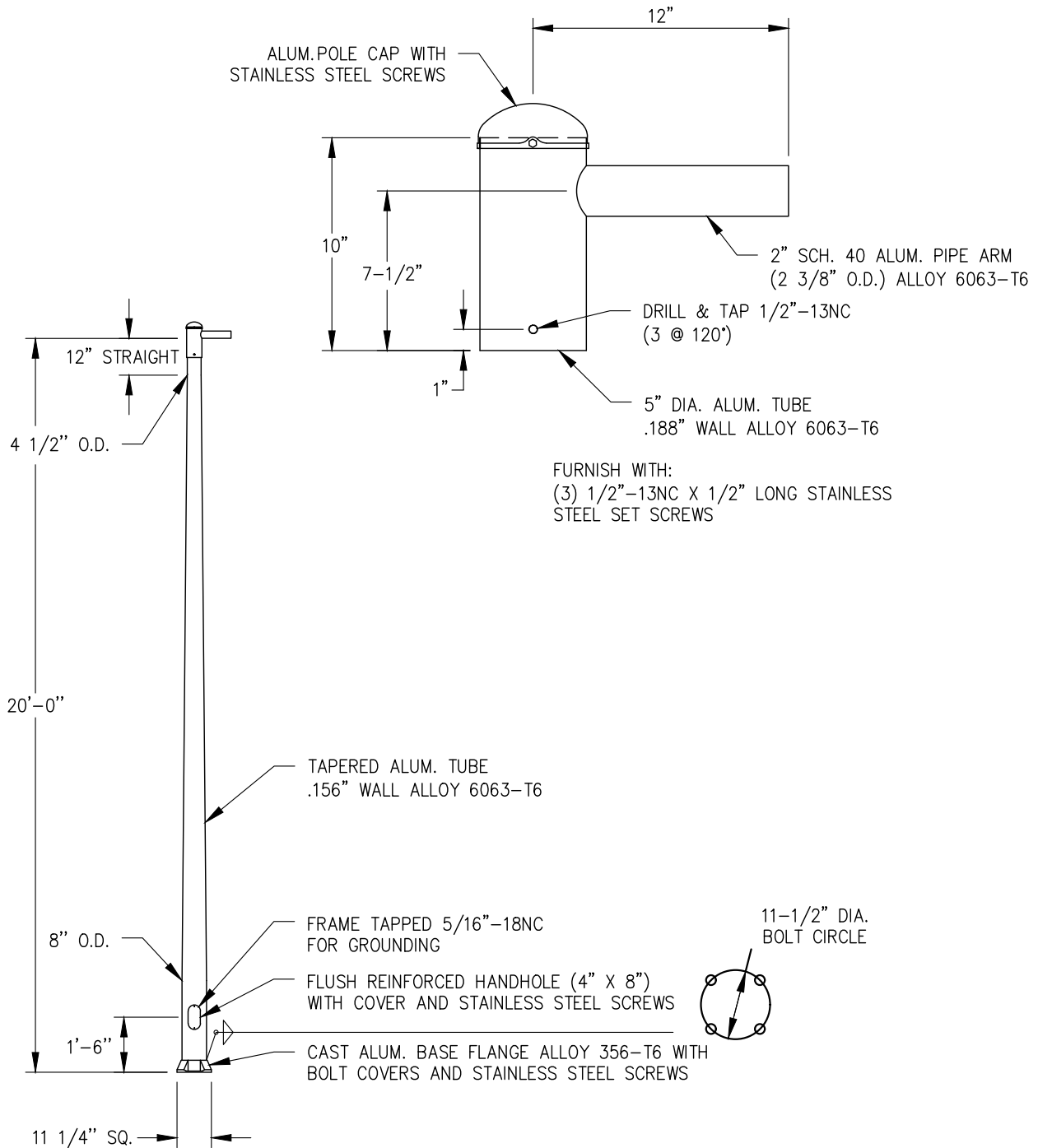


NOTES:
POLE ASSEMBLY SHALL BE POWDER COATED RAL 6005. SEE CITY
STD. SL 50-1 FOR STREET LIGHT FOUNDATION - TYPE A. POLE TO
BE HAPCO B103752 WITH 103991 BASE.

NO.	REVISED	BY	APP.


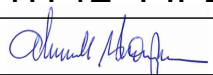
	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DWN: CLG	<h1>YORK SERIES LIGHTING POLE</h1>	SL 11
	CHK JT		PAGE 1 OF 1
DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 12.dwg Layout Name: SL 12 Plot Date: Dec 07, 2021 at 12:40

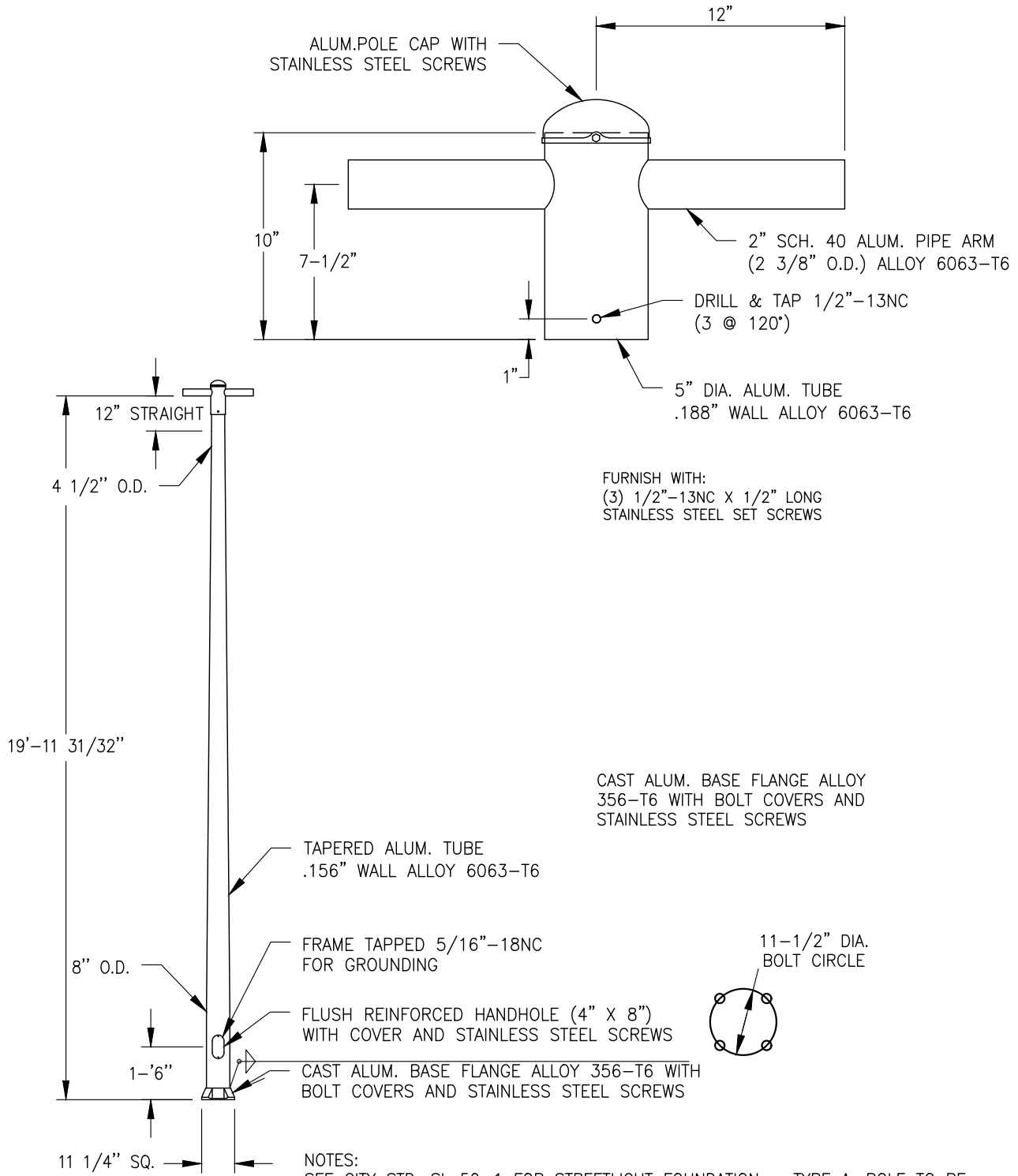


NOTES:
SEE CITY STD. SL 50-1 FOR STREET LIGHT FOUNDATION - TYPE A. POLE TO BE HAPCO B104570 OR APPROVED EQUAL. HUB MOUNT TO BE HAPCO A103777 OR APPROVED EQUAL. DO NOT INSTALL LIGHTING POLES WITHOUT LUMINAIRES.

NO.	REVISED	BY	APP.


	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	CHK JT	20' TAPERED LIGHT POLE WITH 12" PIPE BRACKET ARM		SL 12
	DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21	PAGE 1 OF 1 SCALE: NONE

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 13.dwg Layout Name: SL 13 Plot Date: Dec 07, 2021 at 12:41

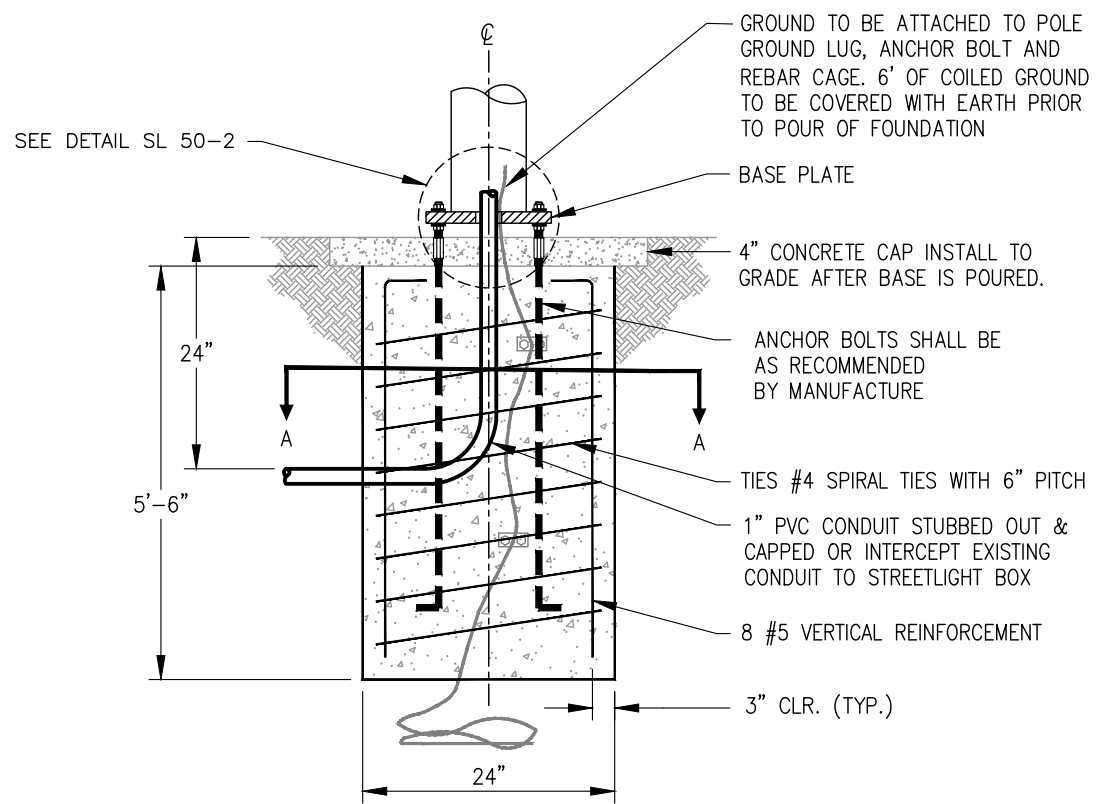


NOTES:
SEE CITY STD. SL 50-1 FOR STREETLIGHT FOUNDATION - TYPE A, POLE TO BE HAPCO B104570 OR APPROVED EQUAL. HUB MOUNT TO BE HAPCO A103777 OR APPROVED EQUAL, DO NOT INSTALL LIGHTING POLES WITHOUT LUMINAIRES.

NO.	REVISED	BY	APP.

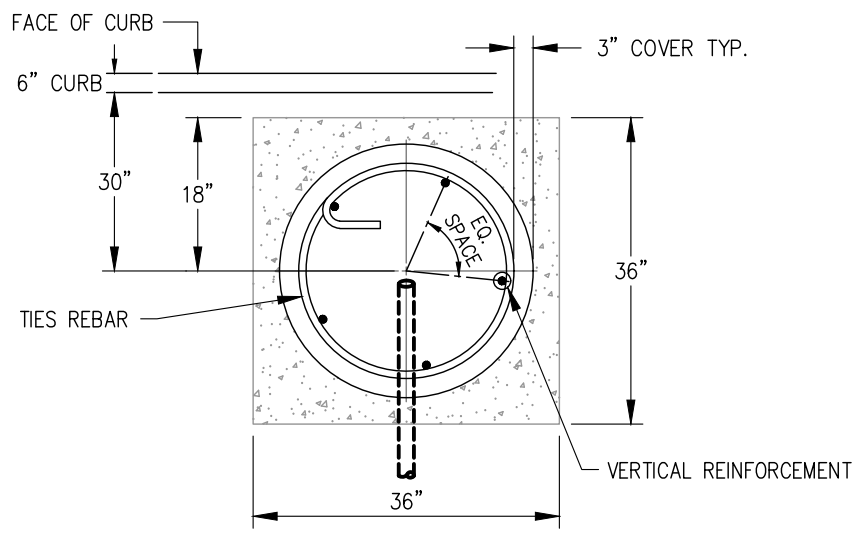
	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	TAPERED LIGHT POLE WITH 12' TWIN PIPE BRACKET ARM		SL 13
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21

Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 50-1.dwg Layout Name: SL 50-1 Plot Date: Dec 07, 2021 at 12:43




NOTES:

1. CALL CITY FOR INSPECTION BEFORE POURING CONCRETE.
2. ALL CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 90-2 "MINOR CONCRETE" OF STATE SPECIFICATIONS, 3/4" AGGREGATE.
3. USE SONO TUBES FOR FORMING THE FOUNDATION.



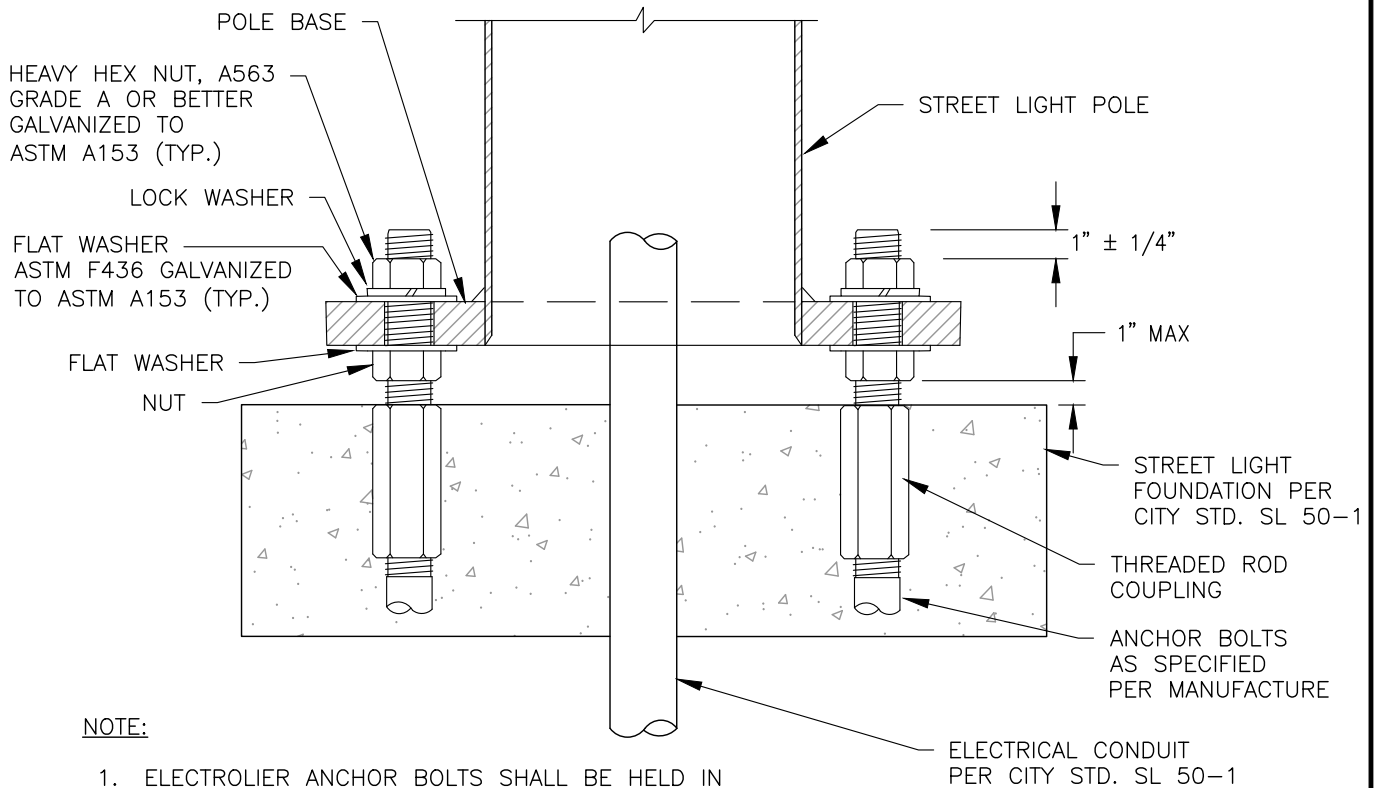
NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	STREET LIGHT FOUNDATION TYPE A	SL 50-1
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i> DATE 12/07/21

PLACE MORTAR
ALL AROUND BOLTS

TOP OF
FOUNDATION


LEAVE CENTER VOID
PROVIDE DRAIN HOLE ALL SIDES



NOTE:

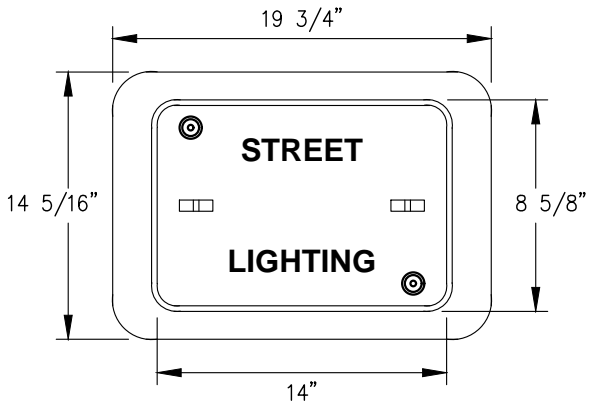
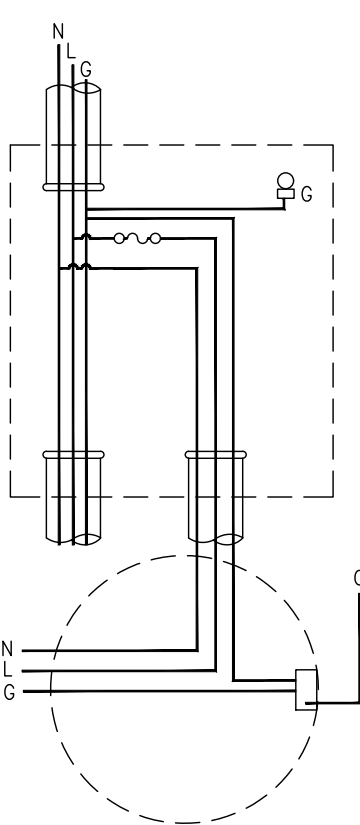
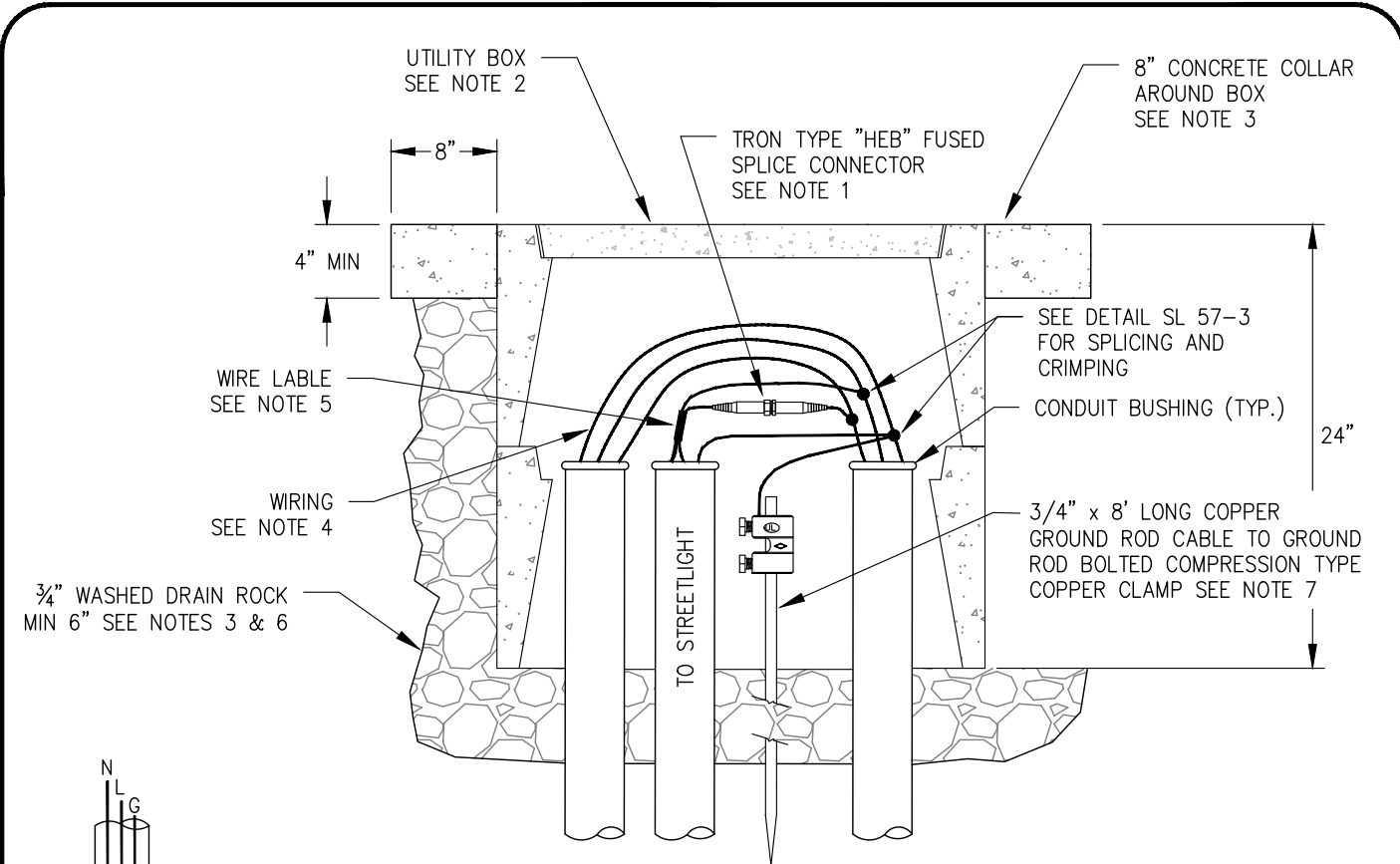
1. ELECTROLIER ANCHOR BOLTS SHALL BE HELD IN POSITION FOR POURING BY MEANS OF A SUITABLE TEMPLATE. DEVIATION FROM THE TRUE POSITION, VERTICAL AND HEIGHT SHALL NOT EXCEED 1/16".

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	STREET LIGHT POLE BASE DETAIL		SL 50-2
	DWN: CLG CHK JT DEC 2021	RUSS THOMPSON CITY ENGINEER	DATE 12/07/21


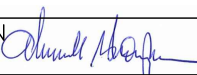
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Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 57-1.dwg Layout Name: SL 57-1 Plot Date: Dec 07, 2021 at 12:48

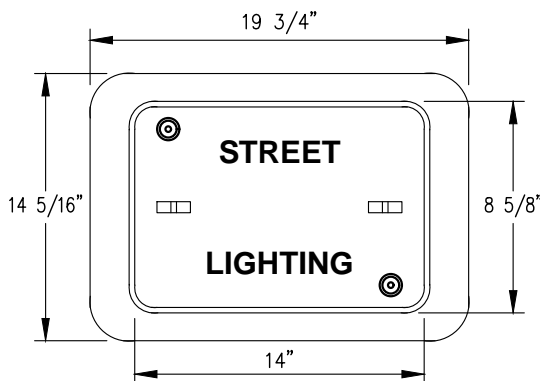
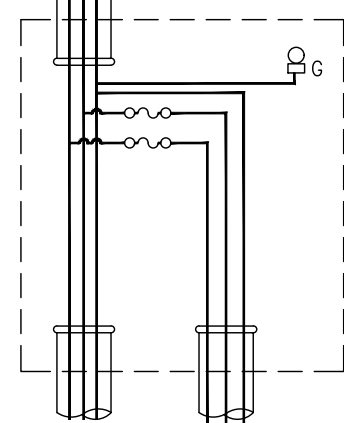
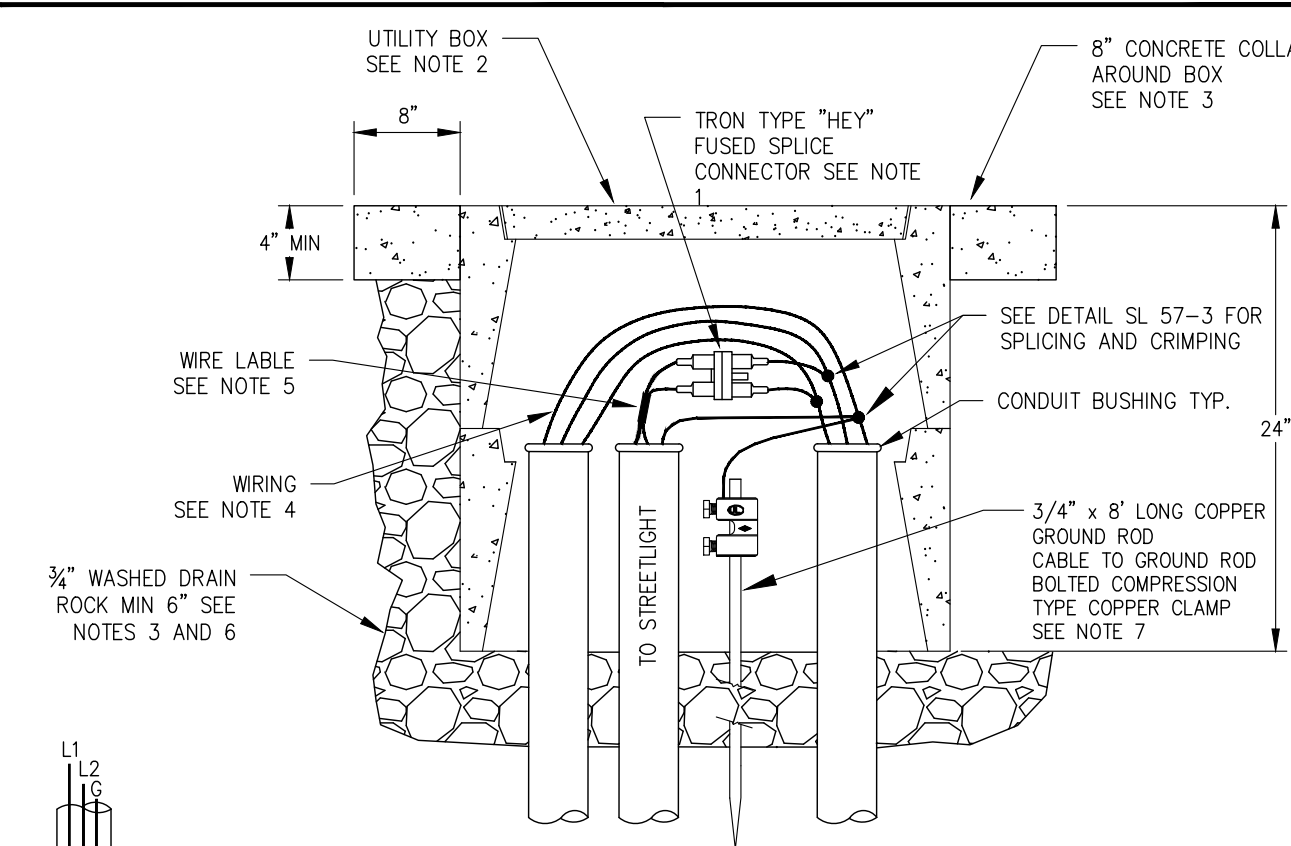


- NOTES:**
1. TYPE HEB FUSEHOLDER, COPPER CRIMP. INSTALL WITH LOAD SIDE TO STREETLIGHT. 3M HEAVY WALL HEAT SHRINK TUBING ITC SN SHALL BE USED ON CRIMPS.
 2. CHRISTY N09 BOX OR EQUAL WITH FIRELYTE BOLT DOWN LID. USE B09X12 FOR EXTENSION.
 3. TO BE APPROVED BY ENGINEER.
 4. ENGINEER TO APPROVE WIRE TYPE, SIZE AND COLOR. MIN. 6' SERVICE LOOP FROM FINISH GRADE.
 5. USE PARTEX POH CARRIER STRIPS WITH YELLOW PK CABLE MARKERS.
 6. WASHED DRAIN ROCK TO BE BOTH UNDER BOX AND ON ALL SIDES WHEN BOX IS LOCATED IN LANDSCAPE.
 7. SEE PLANS OR ENGINEER FOR LOCATIONS OF BOXES TO HAVE GROUND RODS.
 8. PLACE DUCT SEAL AFTER CONDUCTORS ARE PULLED INTO BOX.

NO.	REVISED	BY	APP.

	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT		STD DETAIL
	DWN: CLG	<h2 style="text-align: center;">120V STREET LIGHT WIRING</h2>	SL 57-1
	CHK JT		PAGE 1 OF 3
DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21	SCALE: NONE


Images: Alameda.png; RST_Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 57-2.dwg Layout Name: SL 57-2 Plot Date: Dec 07, 2021 at 12:50



NOTES:

1. TYPE HEY FUSEHOLDER, COPPER CRIMP. INSTALL WITH LOAD SIDE TO STREETLIGHT. 3M HEAVY WALL HEAT SHRINK TUBING ITC SN SHALL BE USED ON CRIMPS.
2. CHRISTY N09 BOX OR EQUAL WITH FIRELYTE BOLT DOWN LID. USE B09X12 FOR EXTENSION.
3. TO BE APPROVED BY ENGINEER.
4. ENGINEER TO APPROVE WIRE TYPE, SIZE AND COLOR. MIN. 6' SERVICE LOOP FROM FINISH GRADE.
5. USE PARTEX POH CARRIER STRIPS WITH YELLOW PK CABLE MARKERS.
6. WASHED DRAIN ROCK TO BE BOTH UNDER BOX AND ON ALL SIDES WHEN BOX IS LOCATED IN LANDSCAPE.
7. SEE PLANS OR ENGINEER FOR LOCATIONS OF BOXES TO HAVE GROUND RODS.
8. PLACE DUCT SEAL AFTER CONDUCTORS ARE PULLED INTO BOX.

NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	240V STREET LIGHT WIRING	SL 57-2
DEC 2021	RUSS THOMPSON CITY ENGINEER <i>Russ Thompson</i>	DATE 12/07/21	PAGE 2 OF 3 SCALE: NONE

3M SCOTCHCAST CONNECTOR SEALING PACK 3570G-N

INSTALLATION

1. THOROUGHLY CLEAN AND DRY THE SURFACE OF THE SUBSTRATE TO WHICH THE MATERIAL IS DESIRED TO BOND.
2. REMOVE GUARD BAG, USING CAUTION NOT TO DAMAGE INNER BAG.
3. GRIP BOTH EDGES OF BAG AT THE CENTER BARRIER (FIG. 1) AND WRINKLE AND FLEX THE BAG ACROSS THE BARRIER. THIS WILL WEAKEN THE BARRIER.
4. SQUEEZE THE CLEAR SIDE OF THE RESIN, FORCING THE RESIN THROUGH THE CENTER BARRIER.
5. MIX THOROUGHLY TO A UNIFORM COLOR BY SQUEEZING CONTENTS BACK AND FORTH 25-30 TIMES.
6. SQUEEZE RESIN TO ONE END OF BAG AND CUT OFF OTHER END. (FIG. 2)
7. SLOWLY INSERT CONNECTION INTO SEALING PACK UNTIL IT FITS SNUGLY AGAINST THE OPPOSITE END. (FIG. 3)
8. WRAP OPEN END OF BAG WITH SCOTCH®SUPER 33+™ VINYL ELECTRICAL TAPE AND POSITION THE TAPED END UP UNTIL RESIN GELS (8-12 MIN. @ 73°F (23° C)). (FIG. 4)

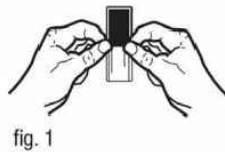


fig. 1



fig. 2

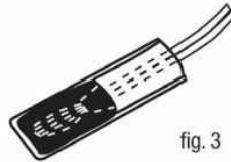


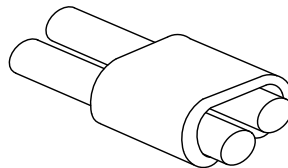
fig. 3




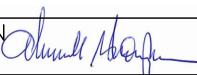
fig. 4

GROUNDING CONNECTIONS

1. ALL ELECTRICAL ITEMS SHALL BE U.L APPROVED.
2. USE GROUNDING COMPRESSION CRIMPING C-CLAMPS/ C-TAPS.
3. USE COPPER WIRE WITH GREEN INSULATION FOR ABOVE GROUND AND WITHIN CONDUITS, SOLID BARE COPPER WIRE FOR BELOW GRADE.
4. USE CORRECT INSTALLATION TOOLING AND DIE SELECTION FOR WIRE SIZE



NO.	REVISED	BY	APP.

	DWN: CLG	CITY OF ALAMEDA, CALIFORNIA, PUBLIC WORKS DEPARTMENT	STD DETAIL
	CHK JT	SPLICING AND CRIMPING	SL 57-3
	DEC 2021	RUSS THOMPSON CITY ENGINEER 	DATE 12/07/21 SCALE: NONE

Images: Alameda.png; fig1.jpg; Fig2.jpg; Fig3.jpg; Fig4.jpg; RST Signature-blue-2.55 INCHES.jpg; Xrefs: Path: F:\BMAP-STD\Alameda\Std\2021 Std Update\SL 57-3.dwg Layout Name: SL 57-3 Plot Date: Dec 07, 2021 at 12:53